

Application Number	Date of Appln	Committee Date	Ward
135565/FO/2022	24th Nov 2022	16 Feb 2023	Deansgate Ward
135583/LO/2022			
135566/LO/2022			

Proposal Demolition of the central atrium, car park ramps, and other elements, retention of casino (Sui Generis), bowling alley and existing food and drink unit (Use Class E) alongside to allow refurbishment, restoration and re-use of the Great Northern Car Park to provide office accommodation, retail and/or food and drink uses (Use Class E) and ancillary areas; and, Refurbishment, restoration, partial demolition and erection of a 3-storey extension to Deansgate Terrace (northern section) and Goods Yard Entrance to form office, retail and / or food and drink use (Use Class E) and drinking establishments / hot food takeaway (Sui Generis), with associated public realm works comprising the reconfiguration of Great Northern Square and the introduction of additional public realm with associated hard and soft landscaping, highways works, access and servicing facilities, rooftop plant and other associated works; and, Partial demolition of the existing leisure box to erect three residential buildings (ground plus 16, 27 and 34 storeys) to provide 746 residential apartments (Use Class C3), reconfiguration of existing car parking together with the creation of office, retail and / or food and drink use (Use Class E), or Sui Generis (drinking establishments / hot food takeaway) at ground floor and level 3, hard and soft landscaping, together with highways works, access and servicing facilities, rooftop plant and other associated works; and,, External alterations and restoration of Deansgate Terrace (southern section) to provide office, retail or food and drink use (Use Class E) or Sui Generis (drinking establishments / hot food takeaway / live music venue), along with delivery of associated public realm, highways works, plant, and associated works

Listed Building Consent for Removal of non-original buildings and structures attached to the former Goods Yard Entrance (223-233 Deansgate Terrace) to facilitate the construction of a new 3 storey extension for office, food and drink, retail and / or Sui Generis (drinking establishments / hot food takeaway) uses and Internal alterations, restoration and construction of a new integrated part 2, part 3 storey extension to the rear 235-291 Deansgate Terrace to facilitate office, retail or food and drink use (Use Class E) or Sui Generis (drinking establishments / hot food takeaway / live music venue) uses, and associated works

Listed Building Consent for internal and external alterations to the Great Northern Warehouse as part of its restoration, refurbishment and re-use to include office, retail, restaurants / food and drink, alongside retention of the existing bowling alley, ground floor restaurant and casino following removal of non-original building elements

Location Land Bounded By Deansgate, Great Bridgewater Street, Watson Street & Peter Street, Manchester, M3 4EN

Deansgate Terraces (223 - 291 Deansgate) Manchester M3 4EN

Great Northern Warehouse Manchester M3 4EN

Applicant GNW Developments Ltd

Agent Mr Michael Percival, Deloitte LLP

EXECUTIVE SUMMARY

The proposal is for planning and listed building consent to refurbish and convert the Great Northern Warehouse into Grade A office accommodation. The Leisure Box complex would be partially demolished and three residential buildings (16, 27 and 34 storeys) constructed to form 726 homes. Deansgate Terrace North and Deansgate Terrace South would be refurbished and altered to form commercial space. New public realm and highway works are proposed

Two objections have been received.

Key Issues

Principle of the proposal and the schemes contribution to regeneration The development accords with national and local planning policies, and would bring significant economic, social and environmental benefits. This brownfield site contains a grade II* and grade II listed buildings. The warehouse is used as a car park which is not an appropriate use and does not best reveal its significance. This building requires significance investment.

The proposal would repurpose the car park for 26,000 sqm of Grade A office accommodation. The Leisure Box would be partially demolished for the development of 726 homes for open market sale. Deansgate Terrace (north and south) would be refurbished to create commercial space and activate new areas of public realm. The homes and office accommodation would be close to public transport, walking and cycle routes. 10% of parking spaces would be fitted for EV charging with a further 10% fitted with infrastructure. There would be 100% cycle provision and enhanced public realm including new Great Northern Square, enhanced north south links, footway and highway improvements.

Economic 3,648 direct jobs would be created during construction. These equate to £124.4 million per year for the 5 year construction period. 850 jobs would be created in the workspaces and commercial floorspace with an annual GVA of £42.8 million per year. This is a significant improvement on the current value of the site which currently creates 384 jobs and £16 million of GVA. The listed buildings would be restored, repaired and repurposed. £2.3 million would be generated per year from business rates. A further £1.4 million per year would be generated from Council tax. The development would also see 1222 new residents at the site who would spend

locally. The average household expenditure is expected to be £28,278 per annum with £11,300 per annum being spent locally.

Social A local labour agreement would ensure that Manchester residents are prioritised for construction jobs. The scheme would stabilise and repair the listed buildings. New public realm would improve connectivity and provide a pedestrian friendly environment in this part of the city centre.

Environmental This would be a low carbon development in a highly sustainable location. It would be highly efficient and meet some of its energy needs through renewable technology. There are no harmful impacts on traffic and local air quality and any impacts can be mitigated. The ground conditions are not complex or unusual and drainage aims to minimise surface water run off including a blue and green roof to the residential podium. The height, scale and appearance of the new building would respect the setting of the listed buildings and conservation area. Secured by Design principles would ensure the development is safe and secure. Waste management would prioritise recycling.

Impact on the historic environment The proposal is viable and deliverable. The refurbishment works would result in less than substantial harm to the significance of the listed buildings. Highly significant features would be retained, repaired and revealed. A clear and convincing justification has been presented to justify the level of harm caused to the building and significant public benefits would be delivered.

Impact on local residents and local businesses The impact on daylight/sunlight, glare and overlooking are considered to be acceptable. Construction impacts would not be significant and can be managed to minimise the effects on local businesses. Noise outbreak from plant and the commercial unit would meet relevant standards.

A full report is attached below for Members consideration.

Description

This 2.97 hectare site is bounded by Watson Street, Peter Street, Deansgate and Great Bridgewater Street. It is in the Great Northern, Manchester Central and Castlefield Quay Strategic Regeneration Area.

The site consists of four main elements:

- Great Northern Square;
- Great Northern Warehouse;
- Deansgate Terrace; and
- Leisure Box Building.

The 'Impossible Bar', located on the edge of Great Northern Square, is not part of this application.

The Great Northern Warehouse is a grade II* listed building (the former Great Northern Railway Company Good Warehouse) which is used for retail, restaurants and leisure including a bowling alley, casino and public car parking.

To the south of the Great Northern Warehouse is an atrium space which acts as a connecting structure providing pedestrian access between the warehouse and the Leisure Box Building. The Leisure Box contains a cinema and multi storey car park.

The Grade II Deansgate Terrace ranges between 2 to 4 storeys in height and has 29 ground floor retail units. The upper floors are office accommodation and storage.

The spaces between the Great Northern Warehouse, Leisure Box and Deansgate Terrace are a mixture of access and servicing including car park ramps to the upper levels of car parking in the Great Northern Warehouse. There are 1240 parking spaces at the site: 519 in the Great Northern Warehouse and 721 in the Leisure Box.



Great Northern Complex and surrounding area

The area is highly sustainable. Deansgate/Castlefield tram stop and Deansgate Train Station are nearby and bus services run along Deansgate.

The Beetham Tower is to the south with St John's Gardens, a mixture of two to four storey residential and commercial properties to the west. To the east is Manchester Central. Peter Street to the north contains a range of bars and restaurants.

The site is not in a conservation area, the Great Northern Warehouse and Deansgate Terrace provide views from and into the Peter Street conservation area to the north. The site is directly east of the Castlefield conservation area which borders the Deansgate Terrace. There is a grade II listed canal that runs below the site that is

unaffected by the proposals. There are 41 listed buildings within 500 metres including a group listing (St Johns Street) that comprises 13 listed buildings.

The site is in Flood Zone 1 and a critical drainage area. It is in the Manchester Air Quality Management Area (AQMA) where air quality conditions are poor.

Planning History

The Great Northern Warehouse was extensively redeveloped in the late 1990s. This introduced the Leisure Box and Deansgate Mews and the refurbishment of Deansgate Terrace (052193/FO/CITY3/97).

In 2018, planning permission and listed building consent was granted for the refurbishment, restoration and re-use of Great Northern Warehouse building as mixed-use development, including apartments, refurbishment, restoration and extension of Deansgate Terrace and Goods Yard Entrance, reconfiguration of Great Northern Square to allow for redeveloped and additional public realm (118568/FO/2017). This permission has been implemented and is extant.

The Proposal

The proposal consists of four main areas:

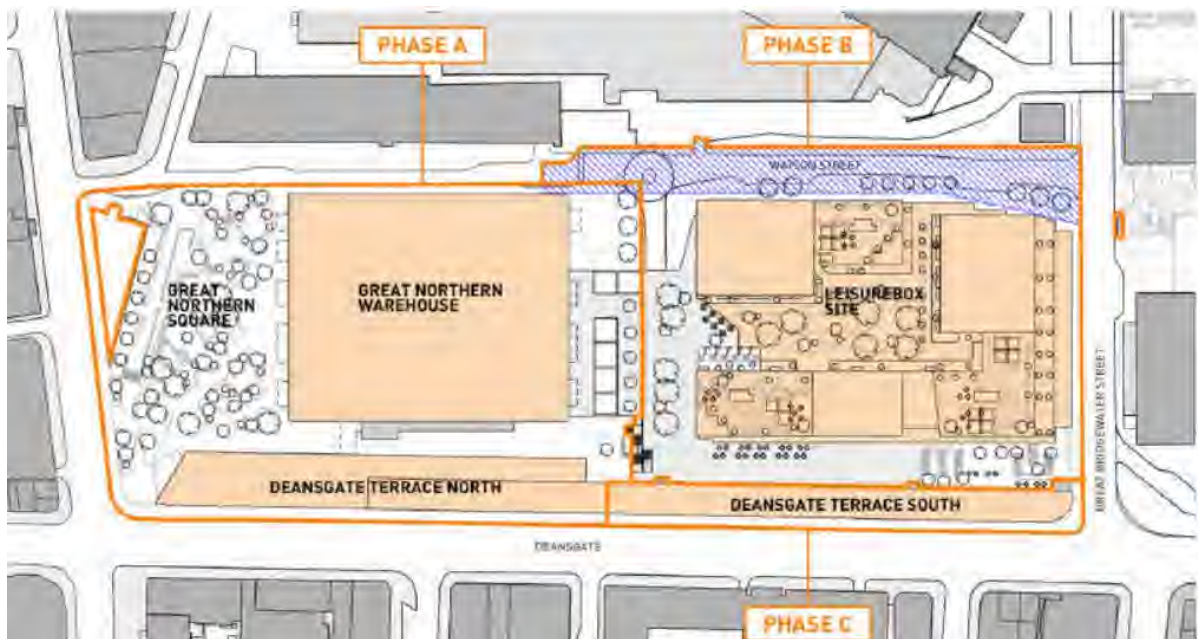
- Retention and alteration of the Great Northern Warehouse;
- Retention and alteration of Deansgate Terrace;
- Removal of the Leisure Box extension, atrium between the warehouse and Leisure Box and modern alterations to the listed buildings; and
- Public realm works.

The alterations and demolitions would allow two areas of public realm to be created – Dean Street and Alport Street. These spaces would include retail, commercial and food and beverage uses.

The applicant intends to deliver the development as a whole. In the event that a continuous development process is not achieved, the development can be split into three phases:

Phase A – refurbishment and conversion of the Great Northern Warehouse including removal of modern features and the refurbishment of the northern section of Deansgate Terrace and enhancement of Great Northern Square

Phase B and C – partial demolition of the Leisure Box and retention of car parking and the erection of the residential element (B) and refurbishment and conversion of the southern end of Deansgate Terrace (C).



Site layout and phases

Great Northern Warehouse

The Great Northern Warehouse would be retained and refurbished. The multi storey car park would be removed and repurposed as Grade A office space. An atrium would be introduced to allow light into the internal spaces. The lower and ground floor commercial space would be refurbished and retained.

Modern additions that facilitate the car park would be removed including the ramps and atrium between the warehouse and Leisure Box. This would allow for the creation of 'Alport Street' and 'Dean Street'.

Deansgate Terrace

The works to Deansgate Terrace would comprise of the following:

- Deansgate Terrace North – demolition of the non-original 1990s structures. Part new build, part refurbishment to create office studio spaces in the upper three levels and commercial at the ground floor with active frontages to Deansgate and Dean Street;
- Deansgate Terrace South – Refurbishment of internal space to provide flexible commercial space in the upper levels with community space and ground floor commercial spaces providing active frontages to Deansgate and Deansgate Mews; and
- Deansgate Mews – refurbishment of the area. The bay to the south of the existing stepped access to Deansgate Mews would form part of the new at grade pedestrian connection through the site. New openings would be created in the east elevation.

Leisure Box

The Leisure Box would be partially demolished, with the lower concrete framed section retained and reused. These retained spaces, accommodates parking and would be expanded to provide additional car parking. The concrete structure would be adapted to accommodate new development above. A two-storey podium would form two additional parking levels and commercial and amenity spaces would provide active frontages to Deansgate Mews and Viaduct spaces. Three separate residential elements (buildings A, B and C) would be constructed on top of the concrete podium to form 746 homes with shared and private amenity areas. An active ground floor use would be created to Watson Street.

Public Realm

The alterations to Great Northern Warehouse and the Leisure Box would allow Dean Street and Alport Street to be created with ground floor units providing retail, commercial and food and beverage uses. Watson Street would be improved to create a pedestrian friendly area supported by active uses.

12,076 sqm of public realm would be created including 156 trees. The public realm works are described as follows:

- Great Northern Square – retention and refurbishment with children’s play equipment;
- Dean Street – a new street to improve circulation and vibrancy allowing appreciation of the western façade of the warehouse;
- Alport Street - The stepped connection from Deansgate to Deansgate Mews would be removed and an at grade route created connecting with Watson Street. The original beams of the viaduct would be revealed;
- Deansgate Mews – Improvements to the north south link via Deansgate Mews;
- Petersfield Works – The works would consider the opportunities with the ‘Viadux Scheme’ to create a pedestrian friendly environment along Watson Street connecting to Great Bridgewater Street.



Visualisation of the refurbished Deansgate Terrace and Greater Northern Warehouse, new residential towers and enhanced Deansgate Square

The Planning Submission

This planning and Listed Building applications have been supported by the following information:

- Design & Access Statement;
- Public Realm Strategy;
- Façade Lighting Strategy;
- Stage 2 Civil and Structural Report;
- Condition Surveys;
- Statement of Community Involvement;
- Archaeological Desk Based Assessment and covering note;
- Environmental Standards Statement including BREEAM Pre-Assessment;
- Ventilation, Extraction and Odour Statement;
- Energy Statement;
- Preliminary Ecological Assessment and Biodiversity Net Gain Assessment;
- Arboricultural Impact Assessment;
- Crime Impact Statement;
- Threat and Risk Security Needs Assessment;
- Transport Assessment;
- Travel Plan Framework;
- Waste Management Strategy;
- Television Baseline Survey Report (pre-development);
- Aviation Impact Assessment;
- Telecommunications Impact Assessment;
- Viability Assessment;
- Place Strategy including Local Benefits Proposal;
- Fire Statement;

- Flood Risk Assessment and Detailed Drainage Strategy (Great Northern Warehouse and Deansgate Terrace);
- Flood Risk Assessment and Drainage Strategy (Leisure Box);
- UXO Risk Assessment; and
- Phase 1 Preliminary Risk Assessment.

The application is also the subject of an Environmental Statement which includes the following chapters:

- Construction management, Demolition and Logistics Plan;
- Townscape and Visual Impact Assessment;
- Historic Environment;
- Air Quality;
- Noise and Vibration;
- Wind;
- Sunlight, Daylight, Overshadowing & Solar Glare;
- Socio-Economics;
- Human Health;
- Climate Change; and
- Cumulative Effects.

Consultations

The proposal has been advertised as a major development, as being of public interest, as affecting the setting of a Listed Building, conservation area and listed building consent. A Site notice was displayed and a notice placed in the local press. Notification letters have been sent to an extensive area of residents and businesses.

Two comments has been received which states that although any positive redevelopment of the area is welcome, there is concern about the size of the tower, its proximity with the Hilton and the GNT and the footprint of the new 746 residential apartments in the area that is already saturated.

The tower would deprive sun light at certain hours of the day and limit visibility, plus the 746 families and car park would have a drastic impact in the accessibility of Watson Street which is already collapsed by traffic and will risk the accessibility of emergency vehicles. All of the apartments would be rentals instead of private owners, which would affect the type of neighbourhood created and impact on noise and vandalism. There would also be impact on the privacy of the apartments in GNT next to the new offices. The development would also loose the gym and cinema which is nice to have in the warehouse.

Consideration should be given to the recent Supreme Court rulings over tall rise buildings overlooking other residential buildings (Tate Modern Case London) and the payment of compensation to those overlooked. Both the owners and the Council potentially will be liable for compensation with any new buildings overlooking other properties.

There would be increased traffic, pollution and noise that almost 1000 extra cars in the area

The proposal would be detrimental to local business already established within and alongside the Great Northern building. Only 2 business locations on Deansgate side that have confirmed the landlord will extend their lease, but the others have said the landlord isn't responding to them, with an indication that they may lose their premises/lease - leading to loss of jobs, impacting during a time of economic hardship and counteractive to economic boosting ideology for Manchester.

Highway Services advise the site is in a highly accessible location. Watson Street would be narrowed, and 3 loading bays altered which will require agreement through the s278 process. A scheme to prevent through movement along Great Bridgewater Street including Moving Traffic Enforcement cameras to enforce the no through route would need to be agreed together with additional survey work to support this to understand where the trips are being displaced on the highway network. The proposal would upgrade footways and plant street trees.

The car park would be reduced by 311 spaces to 929 on the Leisure Box site. 167 (18%) would be allocated for the residential use which is acceptable given the location. 5% of the parking would be for disabled residents. A management strategy should be agreed for these spaces.

A car club space and EV charging would be provided in the car park. There would be 100% cycle provision at the residential development, 156 cycle spaces for the offices and 34 spaces in the public realm. The servicing arrangements are acceptable along Watson Street. A final servicing strategy should be agreed by planning condition.

Environmental Health details of fume extraction and opening hours of the commercial units should be agreed and acoustic attenuation to prevent noise transfer to residential accommodation. Deliveries should be restricted to 07:30 to 20:00, Monday to Saturday. Sunday/Bank Holiday 10:00 to 18:00. A lighting scheme shall also be agreed and details of plant. Details of noise and overheating are acceptable. Final details for waste management for the residential and commercial accommodation should be agreed. The air quality assessment is acceptable subject to the required number of electric car charging points and appropriate dust control measures during construction. Further details are required about ground conditions to ensure suitable remediation proposal as put in place.

Works and Skills Team recommend a condition requiring a local labour scheme.

Flood Risk Management details of a surface water drainage scheme should be submitted for approval with a flood evacuation plan, management regime and verification report.

Greater Manchester Ecology Unity (GMEU) the buildings have negligible potential for bat roosting. An informative should be imposed in the event bats are found. Demolition and vegetation removal should avoid bird nesting season. Biodiversity improvements should be incorporated.

Environment Agency (EA) no comments at the time of writing this report.

Historic England have provided comments on the various elements of the proposal as follows:

The underlying principle of the works to the Great Northern Warehouse is to provide an active use for the building, parts of which are underused, and in danger of declining. The works seek to reverse some of the more insensitive later alterations. Both would better reveal the exceptional significance of the listed building, and therefore they are supportive of the overarching principles of the proposals. The office use is also supported, as it requires limited subdivision, and would allow the historically open nature of the floorplates to be retained.

They do not have concerns with the majority of the proposals. However, the conversion also requires two moderately harmful interventions- the creation of a glazed atrium and of an external glazed vertical circulation area.

The glazed atrium would require the removal of areas of brick infill between the steel frame, and elements of the roof, but would retain the steel frame. This would inevitably have a considerable impact on how the space is read and experienced, and would negatively affect the historic and architectural interest of the building. Historic fabric would be lost, with the associated loss of its evidential value.

However, the depth of the floorplate creates a considerable barrier to conversion, due to the limited light in the centre of the building. It would be difficult to see how the building could function in a meaningful viable use without responding to this problem and the applicant has clearly demonstrated that the area to be altered has been kept to the minimum necessary.

An external circulation core is required, to provide compliant access to all the floors of the building. This would have a harmful impact on its significance, as it would be an additional element of attached paraphernalia. It would further obscure the ability to read the solid rectangular nature of the building's envelope, something which defines its architectural character. Historic fabric would be lost, which would be harmful, even if it is reused elsewhere.

However, the proposed glazed circulation core would undoubtedly be less visually intrusive than the current vehicular ramps. It would also require the loss of less historic fabric than seeking to locate it internally within the heart of the warehouse.

Cumulatively, they therefore conclude that the proposed solution, while resulting in a moderate level of harm, would minimise this more successfully than any alternative means of providing equal access.

The Leisure Box is not of merit in its own right, and has an unsatisfactory relationship with the warehouse and its demolition is not opposed. Its redevelopment offers the potential for some enhancement to the setting of the warehouse. The new development would be away from the warehouse, allowing all four of its elevations to be read, and for the building's monumentality to be better appreciated. It allows for a

sensitive landscaping scheme around the warehouse and to the rear of Deansgate Terrace.

This proposal is considerable in scale, with three towers, which would loom over the warehouse. This impact is important, as part of the warehouse's architectural interest, and an understanding of its historic importance, stems from the fact that it is read as a dominant and imposing building.

Historic England note that the impact of the towers will be partly diluted as they would be read as part of the group of tall buildings that exist, or are under construction, close to the site. However, this does not negate the fundamental concern about the scale of the towers, given the proximity of the tallest tower to the warehouse.

Deansgate Terrace are listed at grade II, and have already had considerable internal alterations. There is no remit for Historic England in respect of these works.

The applications seek to provide a viable new use for parts of the Great Northern Warehouse. The building is currently redundant and undervalued, and is a significant part of Manchester's illustrious history. Historic England are therefore strongly supportive of this in principle. The conversion scheme would result in some harmful interventions. However, they are comfortable that this moderate level of harm is necessary to secure the wider heritage benefits of bringing the building back into a viable use that secures its future.

Historic England have concerns about the impact of the three new tower buildings on the setting of the warehouse. In particular, their scale would adversely affect the appreciation of this building's arresting presence in the city and its special historic and architectural interest.

They believe the current configuration of built form is not the most sensitive way in which the quantum of development could be achieved. The harmful impact on the significance of the Great Northern Warehouse could be considerably lessened if the larger tower block was placed at the south-eastern corner of the site. This would mean that it would be read more directly with the adjacent tall buildings, and would create a stepping down of the built form towards the warehouse.

Greater Manchester Archaeology Advisory Service (GMAAS) the site lies in an area of known archaeological interest. There could be below-ground remains. When the warehouse was built in the late 19th century, 18th and earlier 19th century workers houses, commercial and industrial premises were demolished. Some foundations relating to these buildings may survive, albeit in a fragmentary state, and be exposed during development for the public realm and under the vehicle access ramps.

The potential for Roman remains is low, due to 19th- and 20th-century development disturbance and being on the periphery of the Roman settlement. Given the likely fragmentary nature of buried remains, an archaeological watching brief during development ground works, targeted on areas of potential, would be appropriate in this instance. GMAAS concurs with the conclusions drawn from the desk-based assessments, and recommends site is the subject to intrusive archaeological investigation in advance of development taking place.

Design for Security at Greater Manchester Police the scheme should be carried out in accordance with the Crime Impact Statement which should be a condition.

Health and Safety Executive (Planning Gateway One) advise that clarification is required to understand the composition of the metal cladding and canopy together with routes to the fire fighting shafts.

Aerodrome Safeguarding advise that they have no objections to the proposal subject to an informative in respect of cranes.

Policy

The Development Plan

The Development Plan consists of The Manchester Core Strategy (2012); and Saved policies of the Unitary Development Plan for the City of Manchester (1995). The Core Strategy is the key document in Manchester's Local Development Framework and sets out the long-term strategic planning policies for Manchester's future development.

A number of UDP policies have been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy and saved UDP policies as directed by section 38 (6) of the Planning and Compulsory Purchase Act 2004 unless material considerations indicate otherwise.

The relevant policies within the Core Strategy are as follows:

Strategic Spatial Objectives - The adopted Core Strategy contains Strategic Spatial Objectives that form the basis of its policies, as follows:

Manchester Core Strategy Development Plan Document (July 2012)

The relevant policies within the Core Strategy are as follows:

SO1. Spatial Principles –The proposal would deliver high quality homes, workspaces, commercial and community spaces in a highly sustainable location in the heart of the city centre in a strategic regeneration area.

SO2. Economy – High quality homes in this sustainable location would support economic growth and new commercial and community spaces would support job creation. The construction would create local job opportunities.

SO6. Environment – The development would be low carbon and highly sustainable using up to date energy efficiency measures in the fabric and construction. There would be a travel plan and 100% cycle provision together with a net reduction in on site car parking provision.

Policy SP1 'Spatial Principles – The proposal would improve visual amenity and restore Listed Buildings. This would contribute positively to the street scene and enhance the area.

Policy EC3 'The Regional Centre', Primary Economic Development Focus (City Centre and Fringe and Policy CC8 Change and Renewal– The proposal would provide homes and office accommodation close to all forms of sustainable transport.

Policy CC9 Design and Heritage – The proposal provides high quality buildings through the refurbishment of listed buildings, high quality new buildings and enhanced public realm.

Policy CC10 A Place for Everyone – The proposal would complement the ongoing regeneration of this part of the city centre. There are constraints due the listed buildings. Improved circulation, level circulation spaces is provided within the public realm together with all new accommodation having lift access. Parking would be provided for disabled people.

Policy T1 'Sustainable Transport' – There is access to all public transport modes including tram and rail stations and bus routes. The site is in easy walking distance of amenities in the city centre and those which would be created at the site.

Policy T2 'Accessible areas of opportunity and needs' - A transport assessment and travel plan demonstrate that the proposal would have minimal impact on the local highway network and would encourage the use of sustainable transport.

Policy H1 'Overall Housing Provision' – This is a high-density development on a previously developed site in a highly sustainable location. The range of accommodation sizes is would be 1, 2 and 3 bedroom accommodation. Amenity spaces and cycle and waste management arrangements would ensure this is a sustainable and high quality development.

Policy H2 'Strategic Housing Location' – The proposal would develop a brownfield site in the city centre and deliver good quality accommodation in a highly sustainable area. The fabric would be efficient with sustainable features such as photovoltaics and sustainable drainage are included.

Policy H8 'Affordable Housing' – The proposal would not provide any affordable housing due to viability constraints. This has been independently tested. The viability would be re-tested at an agreed date in the future to determine if the viability has improved and a contribution can be sought.

Policy EN1 'Design principles and strategic character areas' - This high quality scheme would enhance the regeneration of the area.

Policy EN3 'Heritage' - The impact on the historic environment would be acceptable and this is considered in further detail in the report.

EN4 'Reducing CO₂ emissions by enabling low and zero carbon development' – The proposal would have energy efficient fabric. A travel plan and cycle provision is proposed. The fabric would be energy efficient and minimise energy demands.

Policy EN5 Strategic Areas for low and zero carbon decentralised energy infrastructure The development has a robust energy strategy.

Policy EN6 'Target framework for CO₂ reductions from low or zero carbon energy supplies' - The buildings functions would reduce overall energy demands. The building fabric would be high quality and energy costs should remain low.

Policy EN9 'Green Infrastructure' –The soft landscaping and 156 trees would enhance biodiversity and improve green infrastructure.

Policy EN14 'Flood Risk'- A scheme to minimise surface water runoff would be agreed. The design would not exacerbate existing flood risk and the risk to residents has been minimised.

Policy EN15, 'Biodiversity and Geological Conservation' – The site has low potential for bats and the impact on birds can be mitigated.

Policy EN16 'Air Quality' The impact on air quality would be minimised through careful control of construction activities. There would also be a significant reduction in on site car parking. Other measures to minimise the impact of the operations include a travel plan and 100% cycle provision.

Policy EN17 'Water Quality' - Water saving measures would minimise surface water runoff. Historic uses means that below ground contamination could impact on ground water. Remediation measures are required to minimise any risk to below ground water quality. Air quality would not be worsened subject to mitigation.

Policy EN18, 'Contaminated Land' – Ground conditions can be addressed. The former use of the site require extensive remediation and conditions would protect ground water and ensure the site is appropriately remediated.

EN19 'Waste' – The waste management strategy incorporates recycling principles.

Policy DM1 'Development Management' - Careful consideration has been given to the design, scale and layout of the buildings along with associated impacts on residential amenity from loss of privacy and daylight and sunlight considerations.

PA1 'Developer Contributions' states that where needs arise as a result of development, the Council will seek to secure planning obligations. A legal agreement would be prepared to secure a mechanism to review the viability at an appropriate date in the future in order to determine if there has been a change in market conditions to enable a contribution towards affordable housing in the City as required by policy H8.

For the reasons given above, and within the main body of this report, it is considered

that the proposal is consistent with the policies contained within the Core Strategy.
The Unitary Development Plan for the City of Manchester (1995)

The Unitary Development Plan for the City of Manchester was adopted in 1995. However, it has now been largely replaced by the Manchester Core Strategy. There are some saved policies which are considered relevant and material and therefore have been given due weight in the consideration of this planning application. The relevant policies are as follows:

Saved Policy DC7 ‘New Housing Developments’ – The proposal represents a high quality accessible development.

Saved policy DC18 ‘Conservation Areas’ - The impact on the nearby conservation areas is considered in detail in the report.

Saved policy DC19 ‘Listed Buildings’ – The impact on the listed building is considered in detail in the report.

Saved policy DC20 Archaeology states the Council will give careful consideration to development proposals which affect scheduled Ancient Monuments and sites of archaeological interests, to ensure their preservation in place. This is discussed in detail below.

Saved policy DC26, Development and Noise - The impact from noise sources would be minimised and further mitigation would be secured by planning condition.

For the reasons given below, it is considered that the proposal is consistent with the policies contained within the UDP.

Other material policy considerations

The Guide to Development in Manchester Supplementary Planning Document and Planning Guidance (Adopted 2007)

This document provides guidance to help develop and enhance Manchester. In particular, the SPD seeks appropriate design, quality of public realm, facilities for disabled people (in accordance with Design for Access 2), pedestrians and cyclists. It also promotes a safer environment through Secured by Design principles, appropriate waste management measures and environmental sustainability. Sections of relevance are:

Chapter 2 ‘Design’ – outlines the City Council’s expectations that all new developments should have a high standard of design making a positive contribution to the City’s environment;

Paragraph 2.7 states that encouragement for “the most appropriate form of development to enliven neighbourhoods and sustain local facilities. The layout of the scheme and the design, scale, massing and orientation of its buildings should achieve a unified form which blends in with, and links to, adjacent areas.

Paragraph 2.8 suggests that in areas of significant change or regeneration, the future role of the area will determine the character and design of both new development and open spaces. It will be important to ensure that the development of new buildings and surrounding landscape relates well to, and helps to enhance, areas that are likely to be retained and contribute to the creation of a positive identity.

Paragraph 2.14 advises that new development should have an appropriate height having regard to the location, character of the area and specific site circumstances. Although a street can successfully accommodate buildings of differing heights, extremes should be avoided unless they provide landmarks of the highest quality and are in appropriate locations.

Paragraph 2.17 states that vistas enable people to locate key buildings and to move confidently between different parts of the neighbourhood or from one area to another. The primary face of buildings should lead the eye along important vistas. Views to important buildings, spaces and landmarks, should be promoted in new developments and enhanced by alterations to existing buildings where the opportunity arises.

Chapter 8 'Community Safety and Crime Prevention' – The aim of this chapter is to ensure that developments design out crime and adopt the standards of Secured by Design;

Chapter 11 'The City's Character Areas' – the aim of this chapter is to ensure that new developments fit comfortably into, and enhance the character of an area of the City, particularly adding to and enhancing the sense of place.

Manchester Residential Quality Guidance (2016)

The City Council's Executive has recently endorsed the Manchester Residential Quality Guidance. As such, the document is now a material planning consideration in the determination of planning applications and weight should be given to this document in decision making.

The purpose of the document is to outline the consideration, qualities and opportunities that will help to deliver high quality residential development as part of successful and sustainable neighbourhoods across Manchester. Above all the guidance seeks to ensure that Manchester can become a City of high quality residential neighbourhood and a place for everyone to live.

The document outlines nine components that combine to deliver high quality residential development, and through safe, inviting neighbourhoods where people want to live. These nine components are as follows:

- Make it Manchester;
- Make it bring people together;
- Make it animate street and spaces;
- Make it easy to get around;
- Make it work with the landscape;
- Make it practical;

- Make it future proof;
- Make it a home; and
- Make it happen.

Manchester Green and Blue Infrastructure Strategy 2015

The Manchester Green and Blue Infrastructure Strategy (G&BIS) sets out objectives for environmental improvements within the City in relation to key objectives for growth and development.

Building on the investment to date in the city's green infrastructure and the understanding of its importance in helping to create a successful city, the vision for green and blue infrastructure in Manchester over the next 10 years is:

By 2025 high quality, well maintained green and blue spaces will be an integral part of all neighbourhoods. The city's communities will be living healthy, fulfilled lives, enjoying access to parks and greenspaces and safe green routes for walking, cycling and exercise throughout the city. Businesses will be investing in areas with high environmental quality and attractive surroundings, enjoying access to a healthy, talented workforce. New funding models will be in place, ensuring progress achieved by 2025 can be sustained and provide the platform for ongoing investment in the years to follow.

Four objectives have been established to enable the vision to be achieved:

1. Improve the quality and function of existing green and blue infrastructure, to maximise the benefits it delivers
2. Use appropriate green and blue infrastructure as a key component of new developments to help create successful neighbourhoods and support the city's growth
3. Improve connectivity and accessibility to green and blue infrastructure within the city and beyond
4. Improve and promote a wider understanding and awareness of the benefits that green and blue infrastructure provides to residents, the economy and the local environment.

Great Northern, Manchester Central and Castlefield Quay SRF (2017 and 2019 update)

The SRF was originally endorsed by the City Council in 2017 and subsequently updated in 2019. The endorsement of the document highlights the long standing ambition of the City Council to see redevelopment at the Great Northern Complex and provide synergy with other regeneration areas in this part of the City Centre.

The SRF identifies that the Great Northern site underperforms and represents an underutilised part of the City, given its prime location. The use of the listed warehouse together with the quality and function of the spaces in and around the site hinders the vitality of the site.

The SRF highlights the opportunity to enhance and extend the public realm in the area to create a network of spaces together with the function and role of Great Northern Square. This proposal would remove modern additions at the site to create a new north south link across the site, footway improvements around the perimeter and an enhanced Great Northern Square with a focus on families and recreational use.

The appropriateness of the car park within the warehouse is also considered by the SRF. This proposal would repurpose the warehouse for Grade A office space which would allow the listed building to be appropriately repaired to better reveal its significance.

The SRF also states that Deansgate Terrace, and its commercial strategy, should be refreshed. This proposal would refurbish the buildings and provide new active use onto the new public realm.

The car park and buildings in the southern part of the site together with the large atrium that sits between the warehouse and existing cinema is also expected to be removed by the SRF. This proposal delivers upon this ambition through the erection of a series of residential towers to form a distinct form of development at the application site.

City Centre Strategic Plan 2015-2018 (March 2016)

On the 2 March 2016 the City Council's Executive approved the City Centre Strategic Plan which seeks to provide an up-to-date vision for the City Centre within the current economic and strategic context along with outlining the key priorities for the next few years for each City Centre neighbourhood. This document seeks to align itself with the Manchester Strategy (January 2016) along with the Greater Manchester Strategy. Overall the City Centre plan seeks to "*shape the activity that will ensure that the City Centre continues to consolidate its role as a major economic and cultural asset for Greater Manchester and the north of England*".

It should also be noted that the strategic plan approved by the Executive also endorsed an extended boundary of the City Centre upon which the strategic plan is based. This extended boundary includes the application site and the wider Ancoats and New Islington area.

Manchester Strategy (January 2016)

The strategy sets the long term vision for Manchester's future and how this will be achieved. An important aspect of this strategy is the City Centre and how it will be a key driver of economic growth and a major employment centre. Furthermore, increasing the centre for residential is fundamental along with creating a major visitor destination.

National Planning Policy Framework (2021)

The revised NPPF re-issued in February 2021. The document states that the '*purpose of the planning system is to contribute to the achievement of sustainable development*'. The document clarifies that the '*objective of sustainable development*

can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs' (paragraph 7).

In order to achieve sustainable development, the NPPF states that the planning system has three overarching objectives – economic, social and environmental (paragraph 8).

Section 5 '*Delivering a sufficient supply of new homes*' states that *a sufficient amount and variety of land should come forward where it is needed, that the needs of groups with specific housing requirements are addressed and that land with permission is developed without unnecessary delay'* (paragraph 60).

Para 65 states that at least 10% of housing should be for affordable homeownership, unless this would exceed the level of affordable housing required in the area, or significantly prejudice the ability to meet the identified affordable housing needs of specific groups.

This proposal would redevelop a brownfield site and listed buildings in a key regeneration area for 746 new homes. A mixture of 1-, 2- and 3-bedroom accommodation would be provided catering for all family sizes and needs. Viability has been tested and in order to deliver a viable and deliverable scheme to the quality proposed, together with restoring the listed building, the scheme could not support an affordable housing contribution. This is considered in further detail within the report.

Section 6 '*Building a Strong, Competitive Economy*' states that significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development (paragraph 81).

19,926 sqm of Grade A (Class E) office space would be created together with other commercial uses. This would support the much needed office supply in the city centre.

Section 8 '*Promoting Healthy and Safe Communities*' states that *planning policies and decisions should aim to achieve healthy, inclusive and safe places* (paragraph 92).

The proposal would be safe and secure. Cycle parking is provided along with car parking. Disabled residents would have access to parking. New public realm and green infrastructure would be provided which would also link into other nearby schemes.

Section 9 '*Promoting Sustainable Transport*' states that '*significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions, and improve air quality and public health*' (paragraph 105).

In assessing applications for development, it should be ensured that: appropriate opportunities to promote sustainable transport modes can be – or have been – taken

up, given the type of development and its location; safe and suitable access to the site can be achieved for all users; and, the design of streets, parking areas, other transport elements and the content of associated standards reflects national guidance including the National Design Guide and National Model Design Code; any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree (paragraph 110).

Developments should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe (paragraph 111).

Within this context, applications for development should: give priority first to pedestrian and cycle movements, both within the scheme and with neighbouring areas; and second – so far as possible – to facilitating access to high quality public transport, with layouts that maximise the catchment area for bus or other public transport services, and appropriate facilities that encourage public transport use; address the needs of people with disabilities and reduced mobility in relation to all modes of transport; create places that are safe, secure and attractive – which minimise the scope for conflicts between pedestrians, cyclists and vehicles, avoid unnecessary street clutter, and respond to local character and design standards; allow for the efficient delivery of goods, and access by service and emergency vehicles; and, be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations. (paragraph 112)

All developments that generate significant amounts of movement should be required to provide a travel plan, and the application should be supported by a transport statement or transport assessment so that the likely impacts of the proposal can be assessed (paragraph 113).

The site is well connected to all public transport modes which would encourage sustainable travel. There would be no unduly harmful impacts on the traffic network with physical and operational measures to promote non car travel. A travel plan and operational management would be secured as part of the conditions of the approval.

Section 11 '*Making effective use of land*' states that '*planning decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions*' (paragraph 119).

Planning decisions should: encourage multiple benefits from urban land, including through mixed use schemes and taking opportunities to achieve net environmental gains – such as developments that would enable new habitat creation; recognise that some undeveloped land can perform many functions, such as for wildlife, recreation, flood risk mitigation, cooling/shading, carbon storage or food production; give substantial weight to the value of using suitable brownfield land within settlements for identified needs, and support appropriate opportunities to remediate despoiled, degraded, derelict, contaminated or unstable land; promote and support the development of under-utilised land and buildings especially if this would help to meet identified needs for housing where land supply is constrained and available sites

could be used more effectively; and, support opportunities to use airspace above existing residential and commercial premises for new homes. (paragraph 120)

Local Planning Authorities should take a positive approach to applications for alternative uses of land which is currently developed but not allocated for a specified purpose in plans, where this would help to meet identified development needs. In particular they should support proposal to: use retail and employment land for homes in areas of high housing demand, provided this would not undermine key economic sectors or site or the vitality and viability of town centres, and would be compatible with other policies in the Framework; make more effective use of sites that provide community services such as schools and hospitals (paragraph 123)

Planning policies and decisions should support development that makes efficient use of land, taking into account: the identified need for different types of housing and other forms of development, and the availability of land suitable for accommodating it; local market conditions and viability; the availability and capacity of infrastructure and services – both existing and proposed – as well as their potential for further improvement and the scope to promote sustainable travel modes that limit future car use; the desirability of maintaining an area's prevailing character and setting (including residential gardens), or of promoting regeneration and change; the important of securing well designed, attractive and healthy spaces (paragraph 124).

Where there is an existing or anticipated shortage of land for meeting identified housing needs, it is especially important that planning decisions avoid homes being built at low densities and ensure that developments make optimal use of the potential of each site. Paragraph 125 (c) states that Local Planning Authorities should refuse applications which they consider fail to make efficient use of land, taking into account the policies in the NPPF. In this context, when considering applications for housing, authorities should take a flexible approach in applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site (as long as the resulting scheme would provide acceptable living standards).

The proposal would re-use a listed building and redevelop existing buildings. The scale and density of the proposal is considered to be acceptable and represents an efficient use of land. There would be a loss of car parking. The 746 new homes and 19,926 sqm of grade A office space would help meet known housing and regeneration requirements in the city centre. The site is close to sustainable transport infrastructure. A travel plan would encourage the use public transport, walking and cycle routes to the site.

Onsite parking would be provided but the overall objective would be to reduce car journeys. Electric car charging would support a shift away from petrol/diesel cars.

Section 12 'Achieving Well Designed Places' states that 'the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. Being clear about design expectations, and how these will be tested, is essential for achieving this. So too is

effective engagement between applicants, communities, local planning authorities and other interest throughout the process” (paragraph 126).

Planning decisions should ensure that developments: will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development; are visually attractive as a result of good architecture, layout and appropriate and effective landscaping; are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities); establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit; optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public spaces) and support local facilities and transport networks; and create places that are safe, inclusive and accessible and which promote health and well being, with a high standard of amenity for existing and future users and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience (paragraph 130).

Trees make an important contribution to the character and quality of urban environments and can also help to mitigate and adapt to climate change. Planning decisions should ensure that new streets are tree lined, that opportunities are taken to incorporate trees elsewhere in developments, that appropriate measures are in place to ensure the long term maintenance of newly placed trees and that existing trees are retained wherever possible (paragraph 131).

Development that is not well designed should be refused, specifically where it fails to reflect local design policies and government guidance on design. Conversely, significant weight should be given to: development which reflects local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes; and/or outstanding or innovative design which promote high levels of sustainability, or help raise the standard of design more generally in an area so long as they fit in with the overall form and layout of their surroundings (paragraph 134).

The design would be highly quality and complement the distinctive architecture within the area. The buildings would be sustainable and low carbon. Biodiversity, green infrastructure and water management measures are included within the public realm. 156 new trees would be planted.

Section 14 '*Meeting the challenge of climate change, flooding and coastal change*' states that the planning system should support the transition to a low carbon future in a changing climate, taking full account of flood risk and coastal change. It should help to: shape places in ways that contribute to radical reductions in greenhouse gas emissions, minimise vulnerability and improve resilience; encourage the reuse of existing resources, including the conversion of existing buildings; and support renewable and low carbon energy and associated infrastructure (paragraph 152).

New development should be planned for in ways that: avoid increased vulnerability to the range of impacts arising from climate change. When new development is brought forward in areas which are vulnerable, care should be taken to ensure that risks can be managed through suitable adaptation measures, including through the planning of green infrastructure; and can help to reduce greenhouse gas emissions, such as through its location orientation and design. Any local requirements for the sustainability of buildings should reflect the Government's policy for national technical standards (paragraph 154).

In determining planning applications, Local Planning Authorities should expect new development to: comply with any development plan policies on local requirements of decentralised energy supply unless it can be demonstrated by the applicant, having regard to the type of development involved and its design, that this is not feasible or viable; and take account of landform, layout, building orientation, massing and landscaping to minimise energy consumption (paragraph 157).

The buildings fabric would be highly efficient and it would predominately use electricity. The landscaping scheme would include trees and planting, Efficient drainage systems would manage water at the site.

Section 15 '*Conserving and Enhancing the natural environment*' states that planning decision should contribute and enhance the natural and local environment by protecting valued landscapes, minimising impacts on and providing net gains for biodiversity, preventing new and existing development from contributing to unacceptable levels of soil, air, water or noise pollution or land instability and remediating contaminated land.

The high performing fabric of the building would ensure no unduly harmful noise outbreak on the local area. Biodiversity improvements would be provided in the form of trees and landscaping which is a significant improvement based on the current condition of the application site.

Paragraph 183 outlines that planning decisions should ensure that a site is suitable for its proposed use taking account of ground conditions and any risks arising from contamination (a).

There is contamination at the site from the former land uses/buildings. The ground conditions are not usual or complex for this part of the city and can be appropriate remediated.

Paragraph 185 outlines that decisions should ensure that new development is appropriate for its location taking into account the likely effects of pollution in health, living conditions and the natural environment.

There would be some short term noise impacts associated with the construction process but these can be managed to avoid any unduly harmful impacts on amenity. There are not considered to be any noise or lighting implications associated with the operation of the development.

Paragraph 186 states that decisions should sustain and contribute towards compliance with relevant limit values or national objectives for pollutants, taking into account the presence of Air Quality Management Areas and Clean Air Zones. Opportunities to improve air quality or mitigate impacts should be identified, such as through traffic and travel management, and green infrastructure provision and enhancement.

The proposal would not worsen local air quality conditions and suitable mitigation can be put in place during the construction process. There would be a travel plan and access to public transport for occupants of the development along with 20% of the car parking spaces being fitted with electric vehicle charging points.

Section 16 '*Conserving and enhancing the historic environment*' states that in determining applications, Local Planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes, or has the potential to include, heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation (paragraph 194).

In determining applications, local planning authorities should take account of: the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation; b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and c) the desirability of new development making a positive contribution to local character and distinctiveness. (Paragraph 197)

In considering the impacts of proposals, paragraph 199 states that the impact of a proposal on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

Paragraph 200 goes on to state that any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification.

Paragraph 202 states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

The effect of an application on the significance of a non-designated heritage asset should be taken into account in determining the application. In weighing applications

that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset (paragraph 203).

The proposal would result in a degree of harm to the heritage assets. This is considered in detail in the report.

Paragraphs 10, 11, 12, 13 and 14 of the NPPF outline a “presumption in favour of sustainable development”. This means approving development, without delay, where it accords with the development plan and where the development is absent or relevant policies are out-of-date, to grant planning permission unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the NPPF.

Planning Policy Guidance (PPG)

The relevant sections of the PPG are as follows:

Air Quality provides guidance on how this should be considered for new developments. Paragraph 8 states that mitigation options where necessary will be locationally specific, will depend on the proposed development and should be proportionate to the likely impact. It is important therefore that local planning authorities work with applicants to consider appropriate mitigation so as to ensure the new development is appropriate for its location and unacceptable risks are prevented. Planning conditions and obligations can be used to secure mitigation where the relevant tests are met.

Examples of mitigation include:

- the design and layout of development to increase separation distances from sources of air pollution;
- using green infrastructure, in particular trees, to absorb dust and other pollutants;
- means of ventilation;
- promoting infrastructure to promote modes of transport with low impact on air quality;
- controlling dust and emissions from construction, operation and demolition; and
- contributing funding to measures, including those identified in air quality action plans and low emission strategies, designed to offset the impact on air quality arising from new development.

Noise states that Local planning authorities’ should take account of the acoustic environment and in doing so consider:

- whether or not a significant adverse effect is occurring or likely to occur;
- whether or not an adverse effect is occurring or likely to occur; and
- whether or not a good standard of amenity can be achieved.

Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. In general, for noise making developments, there are four broad types of mitigation:

- engineering: reducing the noise generated at source and/or containing the noise generated;
- layout: where possible, optimising the distance between the source and noise-sensitive receptors and/or incorporating good design to minimise noise transmission through the use of screening by natural or purpose built barriers, or other buildings;
- using planning conditions/obligations to restrict activities allowed on the site at certain times and/or specifying permissible noise levels differentiating as appropriate between different times of day, such as evenings and late at night, and;
- mitigating the impact on areas likely to be affected by noise including through noise insulation when the impact is on a building.

Design states that where appropriate the following should be considered:

- layout – the way in which buildings and spaces relate to each other
- form – the shape of buildings
- scale – the size of buildings
- detailing – the important smaller elements of building and spaces
- materials – what a building is made from

Health and well being states opportunities for healthy lifestyles have been considered (e.g. planning for an environment that supports people of all ages in making healthy choices, helps to promote active travel and physical activity, and promotes access to healthier food, high quality open spaces and opportunities for play, sport and recreation);

Travel Plans, Transport Assessments in decision taking states that applications can positively contribute to:

- encouraging sustainable travel;
- lessening traffic generation and its detrimental impacts;
- reducing carbon emissions and climate impacts;
- creating accessible, connected, inclusive communities;
- improving health outcomes and quality of life;
- improving road safety; and
- reducing the need for new development to increase existing road capacity or provide new roads.

Heritage states that Public benefits may follow from many developments and could be anything that delivers economic, social or environmental objectives as described in the National Planning Policy Framework (paragraph 8). Public benefits should flow from the Proposed Development. They should be of a nature or scale to be of benefit to the public at large and not just be a private benefit. However, benefits do not always have to be visible or accessible to the public in order to be genuine public

benefits, for example, works to a listed private dwelling which secure its future as a designated heritage asset could be a public benefit.”

Public benefits may also include heritage benefits, such as:

- Sustaining or enhancing the significance of a heritage asset and the contribution of its setting;
- Reducing or removing risks to a heritage asset;
- Securing the optimum viable use of a heritage asset in support of its long-term conservation.

Other legislative requirements

Section 16 (2) of the Planning (Listed Building and Conservation Areas) Act 1990 (the "Listed Building Act") provides that "in considering whether to grant listed building consent for any works to a listed building, the local planning authority or the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses"

Section 66 Listed Building Act requires the local planning authority to have special regard to the desirability of preserving the setting of listed buildings. This requires more than a simple balancing exercise and case law has considerable importance and weight should be given to any impact upon a designated heritage asset but in particular upon the desirability of preserving the setting with a strong presumption to preserve the asset.

Section 72 of the Listed Building Act 1990 provides that in considering whether to grant planning permission for development that affects the setting or character of a conservation area the local planning authority shall have special regard to the desirability of preserving or enhancing the character or appearance of that area

S149 (Public Sector Equality Duty) of the Equality Act 2010 requires due regard to the need to: Eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by the Act and; Advance equality of opportunity between persons who share a protected characteristic and persons who do not share it. The Equality Duty does not impose a legal requirement to conduct an Equality Impact Assessment. Compliance with the Equality Duty involves consciously thinking about the aims of the Equality Duty as part of the process of decision-making.

Environmental Impact Assessment The applicant has submitted an Environmental Statement in accordance with the Town and Country Planning (Environmental Impact Assessment) (England and Wales) Regulations 2017 and has considered the following topic areas:

- Construction management, Demolition and Logistics Plan;
- Townscape and Visual Impact Assessment;
- Historic Environment;
- Air Quality;
- Noise and Vibration;

- Wind;
- Sunlight, Daylight, Overshadowing & Solar Glare;
- Socio-Economics;
- Human Health;
- Climate Change; and
- Cumulative Effects.

The Proposed Development is an “Infrastructure Project” (Schedule 2, 10 (b)) as described in the EIA Regulations. An EIA has been undertaken covering the topic areas above as there are judged to be significant environmental impacts as a result of the development and its change from the current use of the site as a car park.

The EIA has been carried out on the basis that the proposal could give rise to significant environmental effects.

In accordance with the EIA Regulations, this ES sets out the following information:

- A description of the proposal comprising information about its nature, size and scale;
- The data necessary to identify and assess the main effects that the proposal is likely to have on the environment;
- A description of the likely significant effects, direct and indirect on the environment, explained by reference to the proposals possible impact on human beings, water, air, climate, cultural heritage, townscape and the interaction between any of the foregoing material assets;
- Where significant adverse effects are identified with respect to any of the foregoing, mitigation measures have been proposed in order to avoid, reduce or remedy those effects; and
- Summary, in non-technical language, of the information specified above.

It is considered that the environmental statement has provided the Local Planning Authority with sufficient information to understand the likely environmental effects of the proposals and any required mitigation.

Heritage Significance

*The Great Northern Warehouse Grade II**

The Great Northern Warehouse was built in the mid-1890s, and is a monumental testament to the ambition and wealth of the railway companies, and of the technological brilliance of the Victorians. Constructed around a steel frame, its floorplate covers a massive 267 by 217 feet. The building is clad in red brick, with blue brick bandings and stone detailing enlivening the imposing elevations.

It was constructed as a three-way transport interchange, connecting the canal, road and rail networks via a complex system of hydraulic haulage. This relationship is of particular interest, as it is a unique survival of a three-way railway goods exchange station. More widely it feeds into our understanding of the evolution of the railways in the later nineteenth century, and of the evolution of the city of Manchester.

The building is also striking architecturally, and this contributes positively to its significance. The terrace of buildings spanning from 223- 291 Deansgate were built to screen it from the main thoroughfare. This terrace is constructed in red brick with sandstone dressing, and makes a considerable contribution to the streetscape. It includes a continuous range of shops and offices at the southern end, with the former entrance to the goods station located at the other

The Great Northern Warehouse is listed at grade II*, while the terrace running from 235-291 Deansgate, and the former entrance to the Goods Station, are both individually listed at grade II. These buildings were altered in the later twentieth and early twenty-first centuries. A number of these interventions have not contributed positively to the buildings' significance.

Deansgate Terrace Grade II

Deansgate Terrace was construction by the GNR to create a continuous range of shops and offices, hiding the western aspect of the good station from sight. The terrace is 3 storeys in height with four, 4 storey blocks with intermittently rise above the roof level and have chimneys and stone obelisks.

The terrace is constructed from red brick with poured concrete floors encasing steel beams to form structural down stand beams. The windows were all replaced in the early 2000s replicating the original one over one timber sash windows. Each timber and glass shop front is largely the same. The rear of the shops is plain and utilitarian. In 2000s, a substantial walkway was installed with under croft parking created beneath.

Conservation Area Declarations

Deansgate and Peter Street Conservation Area declaration

Designated in June 1985, due to the architectural and historic interest of a number of buildings and clear groupings of buildings that were the result of commercial growth of the city during the mid 18th century to the early 20th century. The conservation area covers the area surrounding Peter Street and the junctions of Deansgate with Quay Street and Bridge Street. Deansgate forms the longest and straightest street in the city. To the south, railway viaducts in Castlefield cross Deansgate.

Peter Street, and its continuation into Quay Street, is the most important junction in the area. Peter Street contains a number of highly significant listed buildings which form landmarks including Albert Hall (Grade II), Free Trade Hall (Grade II) an Royale Club (Grade II).

Castlefield Conservation Area declaration

Designated in October 1979, the conservation area's boundary follows the River Irwell, New Quay Street, Quay Street, Lower Byrom Street, Culvercliff Walk, Camp Street, Deansgate, Bridgewater Viaduct, Chester Road, Arundel Street, Ellesmere Street, Egerton Street, Dawson Street and Regent Road. The area was extended in

June 1985 by the addition of land bounded by Ellesmere Street, Hulme Hall Road and the River Irwell.

The Castlefield area has evolved over many years and the elevated railway viaducts, canals and rivers create a multi-level environment. It has a mixture of buildings from small scale houses to large warehouses and modern buildings. There are a variety of building materials, which tend to be urban and industrial in character.

Further development can take place that respects the character of the area, and there is room for more commercial property. Ideally, new development should incorporate a mix of uses. The height and scale, the colour, form, massing and materials of new buildings should relate to the existing high-quality structures and complement them. This approach leaves scope for innovation, provided that new proposals enhance the area. The diversity of form and style found in existing structures in Castlefield offers flexibility to designers.

St Johns Conservation Area declaration

St John Street is the only surviving Georgian terraced street in central Manchester, and forms the heart of the conservation area which was designated in November 1970.

The boundary of the conservation area follows Artillery Street, Longworth Street, Camp Street, Culvercliffe Walk, Lower Byrom Street, Quay Street and Byrom Street.

The street remains level along its length from Deansgate to Byrom Street, though the latter slopes gently down to Quay Street. At the west end the view along St John Street was originally terminated by St John's Church, now replaced by a formal garden containing a central memorial. Looking eastwards there is no evidence to indicate that there has ever been a specific focal point to punctuate the view. Georgian properties here have given way to a long row of Victorian shops and offices which screen the former Deansgate Goods Station. This is where Alport (meaning 'the Old Town') was located.

St John Street is a wide street by Georgian standards, and this contrasts sharply with the narrow back streets - Artillery Street, Culvercliffe Walk and Longworth Street - which provided access to the workers' cottages. The linear, dynamic, directional character of these streets also contrasts with the static, tranquil spaces of the gardens, particularly St John's Churchyard. The contrast is also significant in the hard materials of the streets and the soft trees and shrubs in the gardens.

Issues

Principle of the redevelopment of the site and contribution to regeneration

Regeneration is an important planning consideration. The City Centre is the primary economic driver in the City Region and must continue to provide office space, that meets occupier requirements, new homes, for a growing population and commercial and recreational developments.

There is an acknowledged shortage of good quality office accommodation and demand has remained strong post pandemic. As occupational demand grows, good quality products must be brought forward in sustainable locations such as this.

Manchester's population has continued to grow rapidly and is expected to increase considerably by 2030. This, together with trends and changes in household formation, requires additional housing. Around 3,000 new homes are required each year and this proposal would contribute to this need. Providing the right quality and diversity of new housing for the increasing population is critical to continued growth and success.

The Great Northern, Manchester Central and Castlefield Quay SRF seeks to guide future development in this area. The SRF highlights that the site is an underutilised part of the city centre and provides an opportunity to create an enhanced and extended network of public realm linking Great Northern Square, Dean Street, Alport Street, Deansgate Mews and Watson Street. In addition, the listed buildings need to be repurposed, particularly the warehouse which is a car park, with a re-refresh of the commercial uses in Deansgate Terrace. The SRF envisages removal of the modern interventions connecting the listed buildings to allow more modern developments.

The proposal would support the principles of the SRF and the economic growth objectives of the City Centre, by delivering 26,000 sqm of Grade A office accommodation in a car park. This would be a significant contribution to the City's office supply and create jobs. Section 6 of the NPPF states that 'significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development.

746 homes would also be delivered following demolition of the leisure box. There would be one, two and three-bed apartments which would be suitable to families. The sizes would be consistent with the City's space standards with all one bed apartments suitable for 2 people.

Retail and leisure uses would be compatible and would support workers and residents and help to create a sustainable neighbourhood.

The new and improved public realm would create permeability, accessibility and connectivity, across the site, and to nearby developments. This would include new children's play provision at Great Northern Square. The loss of the leisure box would allow the creation of new office, residential, heritage benefits and public realm.

This proposal would form an important catalyst in the regeneration of this part of the city centre. It would re-purpose and enhance the warehouse building and Deansgate terrace. It would help to realise the vision set out in the SRF as underpinned by policies SP1, EC1 and EC3 of the Core Strategy.

Significant economic and social benefits include the creation of approximately 3,648 construction jobs for the duration of the construction. The GVA associated with these jobs would be £124.4 million per year.

When the development becomes operational, 850 jobs are expected to be created at the site. These jobs would have a GVA of £42.8m. There is also expected to be £2.3 m per annum generated through business rates.

The 1222 residents are expected to live at the site. The average household expenditure is predicated to be £28, 278 per annum. Council Tax revenue from the 746 new homes is expected to be £1.4 m per annum.

A local labour agreement would ensure that these economic and social benefits are fully realised.

These socio-economic benefits are significant and would repurpose an underutilised site and support economic and population growth which would create jobs and increase local spending and taxation. There are significant heritage benefits associated with the reuse of the warehouse for offices and upgrading of Deansgate Terrace. This would secure the long term repair and maintenance of these important listed buildings and remove none original alterations.

The development would be consistent with the regeneration frameworks for this area including the City Centre Strategic Plan and would complement and build upon the City Council's current and planned regeneration initiatives. The proposal is therefore considered to be consistent with the National Planning Policy Framework, and Core Strategy policies H1, SP1, EC1, EC3, CC1, CC3, CC4, CC7, CC8, CC10, EN1 and DM1. As such, it is necessary to consider the potential impact of the development.

Affordable Housing

Policy H8 establishes that new development should contribute to the City-wide target for 20% of new housing being affordable and 20% should be used as a starting point for calculating affordable housing provision. Developers should provide new homes that are available for social or affordable rent or affordable home ownership, or provide an equivalent financial contribution.

The amount of affordable housing should reflect the type and size of development as a whole and should take into account factors such as an assessment of local need, any requirement to diversify housing mix and the need to deliver other key outcomes, particularly regeneration objectives.

An applicant may seek an exemption from providing affordable housing, or a lower proportion of affordable housing, a variation in the mix of affordable housing, or a lower commuted sum, should a viability assessment demonstrate that a scheme could only deliver a proportion of the 20% target; or where material considerations indicate that intermediate or social rented housing would be inappropriate. Examples of these circumstances are set out in part 4 of Policy H8.

The application proposes 746 homes for open market sale and 26000 sqm of Grade A office space with commercial and retail floorspace. The delivery of homes and the regeneration of the Great Northern site is a key Council priority.

The proposal would develop a complex site containing a Grade II* and Grade II listed buildings. The Grade II* warehouse is underutilised as a car park and has, along

with the Deansgate Terrace, been unsympathetically altered over the years. A significant opportunity has now emerged to repurpose the listed buildings and remove modern alterations. The demolition of the leisure box is complex as parts of the structure are to be retained.

This development would result in the sensitive alteration, repair and conversion of the listed buildings. The removal of the car park from the grade II* listed warehouse and re-purposed as Grade A office accommodation would provide a long term sustainable use and better reveal its significance. The homes would comply with the Residential Quality guide. New public realm would be enlivened through active frontages. The existing public realm would be enhanced. There would be significant highways works to integrate the development and public realm into the wider area. All these matters have an impact on viability.

A viability report, which has been made publicly available through the Councils public access system has been independently assessed on behalf of the Council. This concludes that the scheme would not be viable if it was to support an affordable housing contribution. A benchmark land value of £12,840,000 is within the expected range based on comparable evidence. The Gross Development Value would be £294,105,452 and development costs would amount to £263,460,984. This would give a profit on costs of 10%.

On this basis, the scheme could not support an affordable housing contribution. This would ensure that the scheme is viable and can be delivered to the quality proposed. The viability would be subject to review at an agreed future date to determine any uplift in market conditions which may improve the viability and secure a contribution towards affordable housing in line with the requirements of policy H8.

Climate change, sustainability and energy efficiency

The proposal would be a low carbon in a highly sustainable location with all forms of public transport nearby. Sustainability principles would be incorporated into the construction process to minimise and recycle waste, ensure efficiency in vehicle movements and sourcing and use of materials.

There would be 929 parking spaces (a reduction of 311 spaces). 10% would be fitted with an electric car charging point with a further 10% being fitted with the infrastructure for a EV point. A travel plan would encourage residents to use public transport to reduce vehicle trips. A secure cycle store would provide 100% provision along with 52 stands within the public realm.

The warehouse would be refurbished, the Deansgate Terrace would be refurbished and extended and the Leisure Box would be a new build. This has influenced the environmental standards and energy strategy.

Great Northern Warehouse The building is Grade II* listed and it is a challenge to restore it and improve its energy efficiency. It is in a poor condition, is not watertight or thermally sealed. The development would improve the fabric by upgrading the glazing and the thermal efficiency at roof level. Its listed status limits works to the wall which would be unacceptable from a heritage perspective.

The strategy is to reduce energy use through passive measures including re-glazing windows. The offices would require a new mechanical ventilation system for heating and cooling through air source heat pumps. This would serve the existing commercial units and the casino. LED lighting would be used throughout the building. An area of roof has been identified for photovoltaic panels for on site renewable energy. A BREEAM excellent rating is being targeted for this element of the development.

Deansgate Terrace The terraces would be restored and extended. They are in good condition with no evidence of structural defects or masonry or stonework repairs. 1990s additions would be removed for the northern terrace and new build elements would be added along with refurbishments to create the office spaces. The southern terraces would be more light touch with refurbishment of internal spaces.

The office spaces would be ventilated either naturally or mechanical heat recovery. Photovoltaics and air source heat pumps would be installed to the northern section of the terrace. A location cannot be found on the southern terrace that would not be harmful to its listed status. A BREEAM excellent rating is being targeted for this element of the development.

Leisure Box The upper section of the Leisure Box would be demolished, with the lower parking retained. Residential towers would be built above this space and consideration has been given as to how the building fabric would be able to reduce its energy consumption through passive means.

Each apartment would be provided with a Mechanical Ventilation Heat Recovery (MVHR) unit with rapid opening vents. Each home would have an electric panel heater. These measures would result in a 15% improvement on Part L (2022) of the Building Regulations which would exceed the requirements of policy EN6. A condition requiring a post construction review would verify that this reduction has been achieved.

156 trees would be planted alongside low level planting and shrubs. Bird and bat boxes are proposed with a blue and green roof on the podium of the towers. These measures would improve biodiversity and provide an efficient drainage system which would minimise the effects of surface water.

Townscape and visual impact Assessment

A computer modelling process has provided accurate images that illustrate the impact on the townscape from agreed views on a 360 degree basis which allows the full impact of the scheme to be understood.

A Townscape Visual Impact Assessment (TVIA), which forms part of the Environmental Statement, has assessed where the proposal could be visible from, its potential visual impact on the streetscape and the setting of listed buildings. The assessment utilises the guidance and evaluation criteria set out in the *Guidelines for Landscape and Visual Impact Assessment (3rd Edition) 2013*. The magnitude of the

impacts (both beneficial and adverse) are identified in the assessment as very large, large, moderate, slight or neutral.

23 key viewpoints (including cumulative impacts shown in wire lines) were considered in the townscape assessment as follows:

Viewpoint 1: Quay Street
Viewpoint 2: Watson Street
Viewpoint 3: Deansgate
Viewpoint 4a: St John's Gardens
Viewpoint 4b: St John's Street
Viewpoint 5: Lower Moseley Street
Viewpoint 6: Liverpool Road
Viewpoint 7: Liverpool Road
Viewpoint 8: Castlefield Urban Heritage Park
Viewpoint 9: Albert Square
Viewpoint 10: St Peter's Square
Viewpoint 11: New Cathedral Street
Viewpoint 12: Manchester Cathedral
Viewpoint 13: Castlefield
Viewpoint 14: Castlefield
Viewpoint 15: Ring Road
Viewpoint 16: Regent Road
Viewpoint 17: Trinity Way
Viewpoint 18: Corporation Street
Viewpoint 19: Picadilly Gardens
Viewpoint 20: Medlock Street
Viewpoint 21: Princess Street
Viewpoint 22: Hulme Park
Viewpoint 23: Chester Road

Detailed assessment on the impact on the following Strategic character areas has been undertaken

- Medieval core
- Retail Core
- Civic Quarter
- Spinningfields
- Chapel Street
- St Johns
- Petersfield
- Castlefield
- Water Street
- First Street
- Hulme
- Corridor Manchester

The effect of the development on the above zones, through an assessment of relevant viewpoints, can be summarised as follows.

Medieval Core is an important gateway and transport hub with Victoria Station and tram stops. It is a major retail and leisure area with the Corn Exchange, the National Football Museum, AO Arena and the Printworks. Its significance as a cultural and public area is notable with the presence of the Cathedral, the ecclesiastical and scholastic centre of Manchester from the 15th century.

To the south and east of the Cathedral Yard are the Corn and Produce Exchange which enclose Exchange Square. These all cluster around the medieval street pattern and are bounded by the curving line of the Cateaton Street, Hanging Ditch, Todd Street, Victoria Station and Hunts Bank approach. To Cathedral overlooks Victoria Street and the River Irwell.

View 12: Manchester Cathedral is a channelled views down the River Irwell corridor. The townscape is mixed and includes the Manchester Cathedral and more modern developments such as the Premier Inn and Ramada Hotel. The view is in the Cathedral conservation area with full appreciation of the Cathedral and the public realm in this area.



Viewpoint 12: Manchester Cathedral (existing left image) (proposed right image) (cumulative)

Retail Core form the main retail streets of Market Street, King Street, St Ann Square and part of Deansgate. The area is a mix of historic and modern buildings which characterise the areas between Cross Street and Deansgate. The river Irwell forms the western boundary and includes the three bridges linking Manchester with Salford.

View 18: is on the interface between the Medieval Core and Retail Core and is focused down Corporation Street. Modern development dominate the view. The proposal would not be visible.



Viewpoint 18: Corporation Street (existing left image) (proposed right image)

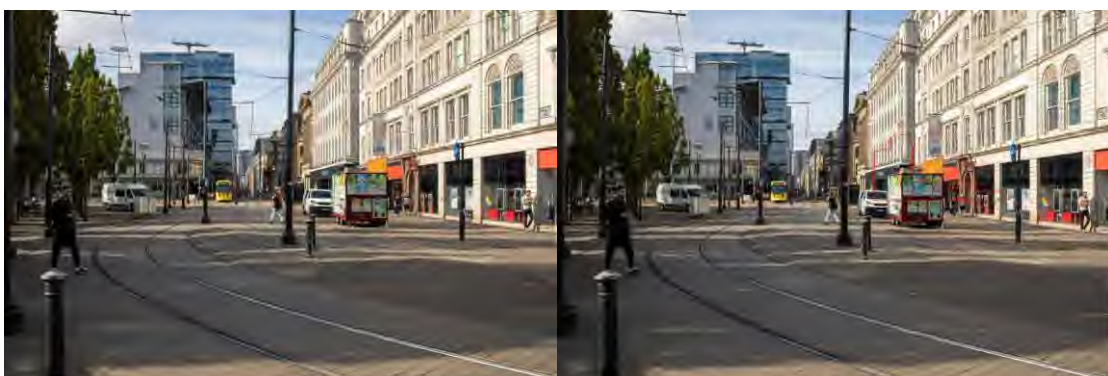
View 11: New Cathedral Street is located in the heart of the retail core on the edge of the St Anns conservation area. Modern developments in the form of the Beetham Tower, Deansgate Square and St Michaels (under construction) are evident in the distant horizon. The listed St Ann’s Church is evident.

This is a sensitive view in the retail core but the proposal would not be visible due to the St Michaels development.



Viewpoint 11: New Cathedral Street (existing left image) (proposed right image) (cumulative)

View 19: Picadilly Gardens offers a niche view down Moseley Street. The townscape is a mix of historic buildings and modern developments. The busy transport interchange and tram infrastructure dominates the view. The proposal is not visible.



Viewpoint 19: Piccadilly Gardens (existing left image) (proposed right image)

Civic Quarter comprises older and modern office buildings with Manchester Town Hall (Grade I listed) and its spire a prominent feature in the skyline. Albert Square is an important civic space. Other notable listed buildings include the City Art Gallery (Grade II*), Central Library (Grade II*) the Midland Hotel (Grade II), war memorial (Grade II), and St Peter's Cross (Grade II).

View 9: Albert Square is currently partially closed due to the Town Hall refurbishment. Nevertheless, it remains an important civic space and its position in the Albert Square conservation area means that the viewpoint is sensitive to change.

The proposal would be visible in between the Beetham Tower and the Great Northern Square building and would reinforce the urban grain in this location. This view would further change as the St Michael's development progresses. The proposal would be absorbed into the view and whilst visible would not be unduly harmful to this sensitive environment.



Viewpoint 9: Albert Square (existing left image) (proposed right image) (cumulative)

View 10: is on the north eastern corner of St Peter's Square with a wide view of the area and is highly sensitive: it is a key civic space and contains views of Central Library, Midland Hotel and the war memorial which are key features of the St Peter's Square conservation area. The Beetham Tower, and other emerging tall buildings, rise above the Midland Hotel.

The proposal sits in the cluster of other tall buildings. Its contemporary design and materiality is evident. Its tone and colour complements that of the Midland Hotel. It would be highly visible in this sensitive view, but merges with the skyline and cluster of other tall buildings.



Viewpoint 10: St Peter's Square (existing left image) (proposed right image) (cumulative)

Spinningfields is a major office destination with a vibrant restaurant scene. New developments dominate the area alongside older buildings such as the Crown and County Courts (Grade II listed), People's History Museum (Grade II), the John Rylands Library (Grade II), Sunlight House (Grade II*) and Masonic Lodge (Grade II). There are new homes along the river Irwell frontage alongside public realm.

View 1: Quay Street is at a busy intersection between Deansgate and Quay Street with a close up view of the site. Deansgate terrace is prominent and The Beetham Tower is a modern development and the Viadux development (under construction) will shortly come into view. It is in the Deansgate conservation area and is highly sensitive alongside the Grade II* listed Great Northern Warehouse.

The towers are evident and the elevations complement the masonry grid of the warehouse. The tallest elements are closest to the Beetham Tower and Viadux forming a cluster of taller buildings away from the listed buildings so not to overwhelm them. The public realm to Great Northern Square is evident and enhances the setting of the listed buildings.



Viewpoint 1: Quay Street (existing left image) (proposed right image) (cumulative)

Chapel Street forms part of the historic core of Salford with new developments taking place in the area. The area includes the Lowry hotel.

View 17: Trinity Way provides a long distance view from which to assess the effects on the Manchester skyline. Trinity Way is one of the busiest road routes in the city.

The Beetham Tower and Deansgate Square are notable tall buildings. The view of the development would largely be obscured by the development of the Trinity Island site which occupies the surface car parks.



Viewpoint 17: Trinity Way (existing left image) (proposed right image) (cumulative)

St Johns The area contains the Grade II Listed Manchester and Salford Junction Canal tunnel, which is underground and the Grade II Listed Colonnaded Railway Viaduct is partially in the area. The area contains non-designated heritage assets. Adjacent listed structures include the Grade I Liverpool Road Station, (Grade I) and buildings at Museum of Science and Industry. The area contains the riverside frontages and a mature garden that faces the existing St. John Street public gardens.

The townscape is experiencing change with significant new developments which has altered the townscape. The river is deeply channelled and largely obscured from view. The Ordsall Chord is to the west.

To the north of Liverpool Rd, and the west of Deansgate, are the low rise Georgian streets of St Johns, home to the chambers and practices of Manchester's legal and medical professions and residential developments in the St Johns Conservation Area. St Johns gardens is a tranquil green space.

View 4a: St John's Gardens is a small quiet public square in the city centre. There are modern developments in the view with the Beetham Tower on the fringes. The view is sensitive as it is located in the St John's conservation area. The towers are visible above the low rise terraces providing a new addition to the skyline.

The scale of the towers are evident but the massing has been broken down to reduce its overall impact. The high quality elevations and architecture is evident which outweighs any harm to the conservation area.



Viewpoint 4a: St John's Gardens (existing left image) (proposed right image) (cumulative)

View 4b: is in the St Johns conservation area and dominated by modest red brick terraces with no modern development meaning it is sensitive to change. Only a small portion of the tallest part of the proposal is visible with glazing being evident in the warehouse. The effect of this view is limited.



Viewpoint 4b: St John's Street (existing left image) (proposed right image) (cumulative)

Petersfield is characterised by high profile venues such as Manchester Central, Bridgewater Hall and the Great Northern Warehouse (grade II*) with a network of open spaces and public realm. Buildings shape and sizes vary with curved roofs to the Bridgewater Hall and Manchester Central contrasting with the vertical and angular nature of the Beetham Tower.

View 2: provide a view of the site with the Great Northern Warehouse clearly evident. The Beetham Tower, Deansgate Square and Viadux would also be evident forming a cluster of tall buildings. The view is outside the conservation area but has a level of sensitivity to change.

The proposal would clearly be evident with the warehouse much improved with new glazing. The tallest towers would be visible but the lower elements would sit behind the existing buildings. The tower would complement the adjacent warehouse due to the composition and quality of its elevational treatment providing visual interest.



Viewpoint 2: Watson Street (existing left image) (proposed right image) (cumulative)

View 5: is outside the Midland Hotel and provides a view of Manchester’s emerging skyline over Manchester Central. There are two listed buildings which add a degree of sensitivity to the view.

The proposal would add positively to the cluster of tall buildings and provide a degree of balance to the Beetham Tower and Viadux developments. Its high quality facades are evident which contrasts with the glazed nature of the Beetham tower.



Viewpoint 5: Lower Moseley Street (existing left image) (proposed right image) (cumulative)

Castlefield is one of Manchester’s key tourist destinations. It is the terminus for the world’s first industrial canal: the Bridgewater Canal and the world’s first passenger railway in 1830. Its historic interest is linked to the historic transport networks formed by the railways and canals. It includes the remaining sections of a Roman fort. It contains offices, apartments, cafes, bars and hotels and includes public spaces and an events area. Castlefield Conservation Area covers this area.

The viaducts offer panoramic views of the city but visually enclose spaces below. These horizontal features contrast with the chimneys and towers associated with the industrial development.

The enclosed canal basins is dominated by its restored heritage with new residential and leisure development. The public spaces and towpaths provide a human scale. The views constantly change along the canals. The Mancunian way passes through

this area with occasional interrupted views of the City centre. A 20-storey development overlooks the basin on Chester Road.

View 3: looks up Deansgate at its intersection with Liverpool Road. It is close range and is dominated by Deansgate Terrace (and its listed façade) and the base of the Beetham Tower.

The proposal would be evident. There would be improvements to the façade of Deansgate terrace and the public realm. The taller elements would be seen in the background to the Beetham Tower providing a contrast in materiality whilst complementing that of the listed buildings.



Viewpoint 3: Deansgate (existing left image) (proposed right image) (cumulative)

View 6: is highly sensitive as it contains the world's oldest surviving terminal railway station which is now part of the Science and Industry Museum. The Beetham Tower and Viaduct are evident and tall buildings form part of its character. The proposal would add positively to this cluster providing a contrasting materiality, colour and tone which would complement the older buildings. The proposal is positive in townscape terms and would be seen in the context of some highly significant listed buildings. This is considered elsewhere within this report.



Viewpoint 6: Liverpool Road (existing left image) (proposed right image) (cumulative)

View 7: is adjacent to the Science Museum and contains a grade II listed building and within the conservation area. The Beetham Tower is evident. The proposal is evident as it rises above the low rise Campfield building and would be highly

prominent. However, it is very much in the background which minimises its overall impact on the skyline and the listed buildings and conservation area.



Viewpoint 7: Liverpool Road (existing left image) (proposed right image) (cumulative)

View 8: contains Roman remains which form part of the park in the conservation area. The Beetham Tower dominates the view above low rise buildings. The tallest part of the proposal can be seen above the tree line. The impact would be minimal.



Viewpoint 8: Castlefield Urban Heritage Park (existing left image) (proposed right image) (cumulative)

View 13: is in the Castlefield basin in the conservation area. The significance of the area is derived from the historical buildings and Bridgewater canal. In recent years, tall buildings have emerged which have formed a cluster in the skyline. The proposal would largely preserve the viewpoint, sitting in the background, behind the Beetham Tower. Contextually the material and fenestrations would complement the older red brick buildings.



Viewpoint 13: Castlefield (existing left image) (proposed right image) (cumulative)

View 14: is in the centre of Castlefield conservation area. The proposal would be more visible than in view 13 but still subservient to the Beetham tower. The proposal is considered to add positively.



Viewpoint 14: Castlefield (existing left image) (proposed right image) (cumulative)

View 23: is at the interchange between the Mancunian Way and Chester Road. The view is not sensitive due to modern developments. The proposal would add to the cluster of tall modern buildings and would form a positive addition to the skyline.



Viewpoint 23: Chester Road (existing left image) (proposed right image) (cumulative)

Water Street is dominated by transport connections including the river Irwell, tram and railway lines (much of which is on viaducts) and the inner ring road. Recent developments include tall buildings at Water Street and Trinity Islands.

View 15: is at the interchange of Trinity Way and the A57. It provides a view over the Manchester skyline but is dominated by road infrastructure. The proposal would be visible adjacent to the taller Beetham Tower. The proposal is a positive addition to the modern buildings and the skyline.



Viewpoint 15: Ring Road (existing left image) (proposed right image) (cumulative)

First Street is the focus of major regeneration activity forming a mixture of commercial, cultural and residential developments including HOME.

View 20: demonstrates the significant development activity which has taken place in this area in recent years. Viadux is the most recent addition to the skyline. The view is susceptible to change and the proposal would add to this albeit largely obscured by Viadux.



Viewpoint 20: Medlock Street (existing left image) (proposed right image) (cumulative)

Hulme has been the subject to a considerable regeneration and a large area of mostly low rise housing has been built. Hulme Park is one of the largest public open spaces close to the city centre. Princess Road which is sunken cuts through this zone and the footbridges that cross over it offer some of the best views of central Manchester. The zone is predominantly residential.

View 22: Hulme Park is an open green space with a dramatic view of the Manchester skyline and the Great Jackson Street regeneration area. Viadux would largely screen the proposal from view.



Viewpoint 22: Hulme Park (existing left image) (proposed right image) (cumulative)

Corridor Manchester is the area between Oxford Road and Cambridge Street and is dominated by educational establishments.

View 21: is on Stretford Road as it crosses over Princess Road and offers a panoramic view over the city centre. The view has seen a significant amount of change and the proposal would be absorbed being largely screened by the Viadux.



Viewpoint 21: Princess Road (existing left image) (proposed right image) (cumulative)

Ordsall is a suburb of Salford consisting largely of low rise residential housing.

View 16: Regent Road is a busy crossing and provides views of the emerging Great Jackson Street SRF area. It is dominated by modern developments with the proposal largely imperceptible.



Viewpoint 16: Regent Road (existing left image) (proposed right image) (cumulative)

The development would form a large and significant development and would transform the Great Northern site and having an overall beneficial impact.

The impact of the height would not be unduly harmful on visual amenity or the city scape. In the majority of instances, the impacts on the local area and on the city townscape would be positive. The high quality architecture and use of materials would create of a distinctive development.

Some visual harm would occur where the development would clearly be seen in the same context as heritage assets. This relates principally to the visual impact on the understanding and appreciation of the setting of the SIM complex (including views along Liverpool Road). However, these impact would not affect the character and appearance of the Castlefield Conservation Areas as a whole.

This low level of harm is outweighed by the substantial regeneration benefits that the development of such a high quality scheme would deliver. This is considered in detail elsewhere in the report.

Impact of the historic environment and cultural heritage

The Great Northern Good Company Warehouse and associated railway yard (now Great Northern Square) is Grade II* listed. The long terrace known as Deansgate Terrace is Grade II listed (two separate listings).



Great Northern Warehouse and Deansgate Terrace

The site is adjacent to the Deansgate/Peter Street conservation area, Castlefield conservation area and St Johns conservation area. There is a grade II listed canal that runs below the site that is unaffected by the proposals. There are 41 listed buildings within 500 metres including a group listing (St Johns Street) that comprises 13 listed buildings.

The warehouse was a large goods depot and warehouse built between 1896 and 1899 by the Great Northern Railway (GNR). The warehouse is rectangular in shape and is five storeys with a basement. The ground and first floor levels are higher than the upper floors as they were originally used for trains.

It has a steel frame with blue engineering bricks to its lower floors with decorative horizontal and vertical banding. There are hard local red brick to the upper floors. The floors are constructed of steel girders, with brick jack arches in between covered in a layer of concrete. Hydraulic hoists, cranes and jiggers were located on every floor, accessed through hatchways.

All of the original 30 pane metal casement windows remain. Some of the original glazing remains whilst louvres were installed in others. The roof consists of six low pitched roofs running west across the building and hipped on all sides. The internal roof structure on level 4 is fully exposed. Each warehouse floor retains its original

jack arched ceiling consisting of many continual barrel vailed brick arches set within a steel framework. This allowed the warehouse to be fully fireproof.

The warehouse was refurbished in 1999 to provide car parking and retail. This resulted in escalators and staircases being added through the original floor structure and partition walls being added. Two large concrete car ramps were erected along the west elevation. At each floor level the car park ramps enter each floor through insertions created by joining to former window openings

Deansgate Terrace was construction by the GNR to create a continuous range of shops and offices, hiding the western aspect of the good station from sight. The terrace is 3 storeys in height with four, 4 storey blocks with intermittently rise above the roof level and have chimneys and stone obelisks.

The terrace is constructed from red brick with poured concrete floors encasing steel beams to form structural down stand beams. The windows were all replaced in the early 2000s replicating the original one over one timber sash windows. Each timber and glass shop front is largely the same. The rear of the shops is plain and utilitarian. In 2000s, a substantial walkway was installed with under croft parking created beneath.

A heritage statement, a detailed design and access statement and structural report examine the condition and impact of the proposal on the significance of the building, its setting and the impact on surrounding listed buildings and conservation areas.

The Grade II* listing reflects its importance as being one of the longest warehouses in length and most advanced at its time. The warehouse has high architectural, artistic and historic interest through its Victorian engineering being an early example of a steel frame building and multi level goods station. It has group value with Deansgate terrace. The most highly significant elements are its original metal framed casement windows and the original glazing where they exist, brick vaults on steel beams and columns, brick jack arched ceilings, open plan warehouse floors, central supporting cross wall to level 4, exposed timber roof and double height space to level 4 and its robust and regular fenestrations to the exterior.

The proposal would result in extensive repairs and alterations to the exterior and interior of the building responding to the high areas of significance identified.

Modern signage advertising the car park, CCTV, lighting and non original rainwater goods would be removed and the building made good. This would remove clutter from the building and repair the elevations. Original rainwater goods would be refurbished and restored. The impact of this work would be beneficial.

The original steel windows would be repaired, cleaned and reglazed and made fully operational. Where louvres have been installed, they would be removed, and the windows restored. Non original openings for the car park ramps would be infilled and replica steel windows installed. This would work would be beneficial to the building and its significance.

The modern canopy and late 1990s extensions would be removed. This would reveal the western façade which would be beneficial and allow all four elevations of the building to be appreciated and understood.

The condition of the masonry varies and would be cleaned and pointed with lime mortar. Missing or deteriorated brick work and stonework would be replaced. The original hoists, which have been filled with non original brickwork, would be re-opened, repaired and replica windows installed. This work would be beneficial.

Girders and columns to the viaduct would be repaired and cleaned and non original features removed. The impact of this work would be beneficial.

The non-original recessed plaza entrance and shop fronts would be upgraded. This would apply to the ground and first floor entrances. The lack of significance of these more modern elements is judged to be moderately beneficial to the significance of the listed building.

The most significant external interventions are a glazed entrance to the south elevation allowing for level access from the warehouse onto podium/upper Dean Street and an external lift core and glazed link bridge to Deansgate terrace. The lift core and bridge would require 6 original steel frame windows and cills to be removed. These windows will be reinstated where the car park ramps were installed. This would cause a minor/moderate adverse impact on the significance of the listed building principally from the visual impact of the structures. However, the glazing would allow the original façade to be seen. The change could be reversed if lift core and bridge were to be removed.

Internally, at the ground floor, the non original plaza entrance would be refurbished and the jack arch ceiling exposed. Two lift cores would be installed south of the escalators with reconfiguration of internal spaces. This would have limited impact on historic fabric and the significance of the building.

The original concrete staircase would be refurbished and the internal brickwork would be repaired, cleaned and exposed. Modern fixtures, fittings, M & E, modern structures/partitions and plant would be removed from the buildings. The impact of this work would be beneficial.

The second, third and fourth floors would be converted into offices. The amount of subdivision would be minimised in order to retain the highly significant open plan space. Any subdivision would involve light weight partitions. This would cause a minor/moderate adverse impact on the significance of the listed building principally from the loss of openness which would prevent the full appreciation of the special character of the floors. This would be reversed if partitions were removed. The conversion of the warehouse to offices would cause less intensive subdivision than the extant planning permission to residential use.

An atrium and rooflights would be installed on the fourth floor to allow light into the deep floorplates below. This would require the removal of original roof structure which would cause a degree of moderate harm. An atrium was proposed as part of

the extant residential planning permission and the atrium subject to this application is smaller minimising the degree of harm to the listed building.

The works to the listed building are extensive in order to convert it into a commercial space. This would cause harm to the listed building and its significance which would only be reversed if modern interventions are removed. The most notable are the subdivision of the large open plan floor plates, installation of the atrium to the roof and erection of the external lift core and bridge.

The commercial use of the building does, however, allow the extent of subdivision to be minimised and where partitions are proposed they are lightweight. This would allow the openness of the floorplates to be as legible as possible minimising the overall degree of harm to the listed building. There would be an extensive programme of repair and restoration works which seeks to retain existing features in situ.

The significance of Deansgate Terrace is derived from its unique and continual form within its regular rhythm of its fenestration, shopfronts and architectural detailing. The arched entrance into the former goods yard at the junction of Peter Street and Deansgate is in the southern part of the Peter Street conservation area. The most highly significant elements of the terrace include its original brickwork and stone detailing and internal finishes.

The refurbishment and conversion of the terrace requires repair, alteration, removal and demolition at the complex and a conservation led approach has been taken. Demolition at the northern part of Deansgate terrace would remove modern additions which would be replaced with a more appropriate extension. The southern part of the terrace would be sensitively restored.

Modern signage, CCTV, lighting and non original rainwater goods would be removed and the building made good. This would remove clutter and repair the elevation. Original rainwater goods would be refurbished and restored. The impact of this work would be beneficial.

The original timber windows would be repaired, cleaned and reglazed and made fully operational. The original roof covering would be removed and an insulated roof construction reusing the original slates. The condition of the masonry varies and would be cleaned and pointed with lime mortar. Missing or deteriorated brick work and stonework would be replaced. The impact of this work would be beneficial.

The most significant interventions externally are the glazed link bridge to the warehouse and installation of the roof terrace. These would cause a minor/moderate adverse impact on the significance of the listed building principally from the visual impact of the structures. However, the glazed nature would allow the original façade to be seen. The change could be reversed.

Consideration has been given to the impact of the Leisure Box redevelopment on the setting of the listed buildings other listed buildings in the surrounding areas and the conservation areas. The heritage statement has considered 14 views to understand the visual impact of the development of the identified heritage assets.

View 1 is from the junction of Deansgate/Peter Street/Quay Street and allows the relationship between the listed buildings to be understood. The converted warehouse would be highly visible and would be beneficial with its repaired and reinstated windows. The repair, alterations and extension to Deansgate terrace north would be visible. The improved public realm would have a positive impact on the conservation area and the setting of the listed buildings.

The redeveloped Leisure Box site would be highly visible. The height of the towers would form a striking feature emerging in the backdrop of the warehouse and terrace. This would alter the setting of the listed buildings, causing a degree of harm but the listed building would remain legible and understood. The high quality architecture of the buildings would be evident including the regular fenestration and crill window style. The development would be read alongside other taller buildings either built or emerging in this area.

View 2 is from Peter Street looking towards the site and demonstrates the prominence of the warehouse. It demonstrates the benefits to the listed building and the conservation area of converting and repairing the warehouse. The redeveloped Leisure Box would form a tall vertical feature behind the warehouse. The scale of the towers would be seen in the setting of the building and cause a degree of harm. The warehouse would remain legible and understood with the taller tower forming the backdrop. The view demonstrates that taller building can be accommodated in the area where they are high quality in nature.

View 3 is experienced from the southern part of Deansgate allowing Deansgate terrace to be understood and its relationship with the edge of the Castlefield conservation area. There are modern buildings in the view. The redeveloped Leisure Box forms a taller element against the backdrop of the terrace. The towers would be seen within the setting of the building causing a degree of harm. However, the massing of the building has been minimised and steps away from the listed building and wider street scene.

View 4a is from St Johns Gardens in the St Johns conservation area. The view is dominated by mature landscaping and trees. The northern most tower would be particularly visible seen above the low rise Georgian buildings. This would erode the setting of the conservation area to a degree particularly the intimacy provided by the low rise buildings in this part of the conservation area. A degree of harm has already been accepted as a result of other taller buildings which are emerging in this view.

View 4b looks east along St John Street. Deansgate Terrace terminates the view. The Grade II listed Georgian townhouses dominates this view enclosing the street. There would be a glimpsed view of the development which would encroach above the roofline of the listed townhouses. This would have a minor impact on the setting of the listed buildings.

View 5 is from Lower Mosley Street. The Grade II* Manchester Central dominates the view. The proposal would be highly visible to the right of the Beetham Tower. It would rise above Manchester Central causing a degree of harm to the setting of the listed building. However, the distinctive arch roof would remain legible and understood.

Viewpoint 6 is from the western end of Liverpool Road outside the former entrance to the Grade I listed Liverpool Road Railway station. The proposal would be visible terminating at the end of the view and clustering with other tall buildings including the Beetham Tower. The development would result in visual change to the view but would preserve the setting of listed buildings and the Castlefield conservation as a whole.

View 7 is from further east along Liverpool Road allowing clear views of the grade II listed Science Museum. The proposal change the view being visible above the roof line of the lower rise listed building. Whilst the listed building would remain legible, dominating this section of Liverpool Road, the proposal would form a large feature in the street scene and within the conservation area. The high quality nature of the façade would be evident and worthy of a tall building in this part of the city centre.

View 8 is experienced from Castlefield Urban Park. The proposal would be seen just above the tree canopy but does not alter the spaciousness of the park. The development would be read within the cluster of taller buildings in the view.

View 9 is from Princess Street. This is a highly sensitive view over Albert Square in the conservation areas. The Grade II listed Town Hall is evident. The proposal would be visible above the roofline of the listed buildings on the southern side of Albert Square. The development would integrate into the skyline of other tall buildings minimising the impact on the Town Hall and Albert memorial. The proposal would not impact on the Albert Square conservation area as a whole which would remain legible and understood.

View 10 is from Moseley Street. The Grade II* Midland Hotel, Town Hall Extension and Central Library are all visible. The backdrop is a number of tall buildings such as Axis, Deansgate Square Towers and Beetham Tower. The proposal would rise above the listed buildings, particularly the domed roof of the library and erode the setting of the listed buildings to a degree. The massing of the building is chamfered which minimises the impact on the roofline of the library. The development as a whole would be read within the cluster of tall buildings.

View 11 is from the norther boundary of St Ann Square conservation area. The grade II Royal Exchange is in the view as is the bell tower of the Grade I listed church of St Ann. The proposal would be form a contemporary backdrop alongside the other tall buildings visible. The relative distant between the listed buildings, conservation area and the development would minimise the harm of the development on the setting and significance of these assets.

View 12 is from the north end of Deansgate in the Cathedral conservation area and is dominated by The grade I Cathedral. The proposal is not visible resulting in a neutral impact on the listed building and the conservation area.

View 13 is from the Castlefield conservation area and is dominated by the grade II former Merchants warehouse. The Beetham Tower is visible. The proposal would be largely obscured by the Beetham Tower. It would add to the grain of development in the area and form a distant building in the setting of the conservation area.

The views demonstrate that impact the restoration of the warehouse and Deansgate terrace would have. Windows and fabric would be repaired and unsympathetic alterations removed. The redevelopment of the Leisure Box would, in some instance, have a minor impact on the setting of conservation areas and listed buildings including the warehouse and Deansgate Terrace.

This would result in *less than substantial harm* to the listed buildings and conservation areas as a whole and therefore the test set out in paragraph 202 of the NPPF apply.

Historic England have not raised any objections to the conversion and repair of the warehouse and Deansgate Terrace but have raised some concern with regards to the scale and massing of the redeveloped Leisure Box.

The conversion and extensions and modifications of the warehouse would cause harm to its significance. This would be overall minor and would allow the internal and external aspects of the building to remain legible and understood. The most highly significant features of the listed buildings would be repaired and restored with modern and less significant interventions removed.

The redeveloped Leisure Box would be a large and significant development in the setting of the nearby listed buildings and the conservation area. The scale, massing, appearance and use of materials would provide a contemporary development that would be clustered to other nearby tall buildings. There would be gaps between the buildings and the listed buildings allowing them to remain legible and understood. There would be localised instances of harm to views of the site from these views.

The proposal would result in a low level of *less than substantial harm* as defined by paragraph 202 of the NPPF, to the significance and setting of the Grade II* and Grade II listed buildings and conservation areas. As directed by paragraph 202 of the NPPF, it is now necessary to consider whether the public benefits required exist which outweighs any this harm. These public benefits will be considered in detail below.

Assessment of Heritage Impact

The proposal would create instances of less than substantial harm as defined within the NPPF. Any level of harm should be outweighed by the public benefits that would be delivered in accordance with the guidance provided in paragraph 202 of the NPPF. In assessing the public benefits, consideration has been given to paragraph 8 of the NPPF which outlines the three dimensions to achieve sustainable development: economic, social and environmental.

The redevelopment and regeneration of this brownfield site is in line with Council policy and would bring 746 homes in a highly sustainable area along with 26,000 sqm of Grade A office space.

The key views demonstrate how the development would have a largely beneficial impact on the majority of views with the exception of some localised views where the development would be highly visible within the setting of key heritage assets.

Whilst the building would be large, it would not be out of context with other tall buildings in this part of the city centre. There would be heritage benefits associated with the conversion of the Great Northern Warehouse and repair of Deansgate Terrace including removal of previously unsympathetic extensions and alterations.

There would be £6 million on improvements to public realm at the site which would include the planting of 156 trees, creation of a family friendly space in the Great Northern Square and improved pedestrian connectivity at the site together with highway improvements.

3,648 full time equivalent jobs would be created during construction. The GVA associated with these jobs would be £42.8 million per year (of a 5 year build programme).

The homes could accommodate up to 1222 residents creating an additional £8.4 million of expenditure to the local economy per annum. Council tax would equate to £2.3 million per annum.

The new workspaces and commercial space at the development would create 850 jobs in total with annual GVA of £38.179 million per year (from £16.023 currently).

The new build would be a low carbon and the warehouse would be as energy efficient as possible. An all-electric system would benefit from a decarbonising grid. Photovoltaic panels to the roof of the warehouse and terrace would generate on site energy. 20% of the parking spaces would be fitted with electric car charging points (or infrastructure). 100% cycle provision would be available.

The significant public benefits would outweigh the heritage impacts which would be at the lower end of less than substantial harm.

It is considered, therefore, that, notwithstanding the considerable weight that must be given to preserving the setting of the listed buildings as required by virtue of S66 of the Listed Buildings Act. The harm caused would be less than substantial and would be outweighed by the public benefits of the scheme and meet the requirements set out in paragraph 202 of the NPPF.

Impact on Archaeology

There is archaeological interest mainly in relation to 18th and early 19th century workers houses, commercial and industrial premises. A watching brief should be maintained with regards to Roman remains which are likely to be low given historical development in the area subsequently. The archaeological investigations should be carried out in accordance with the scheme which has been submitted and agreed by GMAAS. This should form a condition of the approval and would satisfy the requirements of policy EN3 of the Core Strategy and saved UDP policy DC20.

Layout, scale, external appearance and visual amenity

The development would deliver the objectives of the SRF including improving the street level environment, creating high quality public realm and high quality buildings.

The SRF identifies the refurbishment and re-use of the Great Northern Warehouse as a priority. Grade A offices would replace the parking with the ground floor and first commercial units retained. The removal of the spiral staircase and ramps on the west elevation would allow a new pedestrian connections to be created between the warehouse and Deansgate Terrace (Dean Street). The Link building between the warehouse and Deansgate Terrace would be removed.

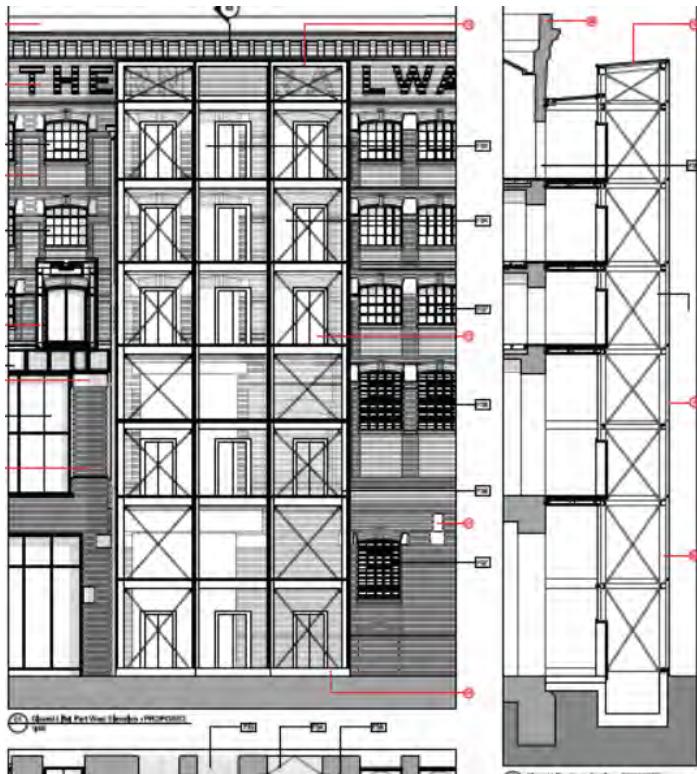
A glazed central atrium would provide natural light into the central part of the warehouse which is essential due to its deep floor plan. This is consistent with the principles established under the 2018 planning permission.

A glazed lift core would be added to the west elevation of the warehouse. A number of options were explored but the external core would have the least impact on the listed building and would be positioned in the location of a number of existing hatch openings which historically would have been used to transport goods into the building and between floors. This would be a lightweight addition to the building allowing the original façade to remain visible. It would not be visible from Deansgate.

A glazed link bridge would re-establish an historical connection between the warehouse and Deansgate Terrace adjacent to the glazed lift core.



Image of the refurbished warehouse and glazed bridge and lift core



Elevation and section of the glazed lift core

The ground floor commercial uses in the warehouse (casino, bowling alley and restaurant) would be retained along with the central mall space which would be refurbished. Platform lifts would be added to the first floor to improve accessibility.

The corner restaurant would be converted into an office reception space and entrance. This would enable access to the new lift. A secondary tenant/staff entrance would be created in the south eastern corner of the building. A new retail unit on the western elevation would activate Dean Street.

The north and south sides of the viaduct would be infilled with commercial units which activate Dean Street and Alport Street. Six bays of the brick arch deck would be removed to provide natural light to the route. A landscaped public space would be created on the upper level of the viaduct.



Image along Dean Street/Alport Street



Image along Dean Street/Alport Street

At the first floor, a flexible commercial space would be created in the location of a retail unit.

The remaining upper floors of the warehouse would be converted to offices. The deep floor plate limits the amount of natural light into the space. A central atrium would be created which would be smaller than previously approved when the warehouse was to be converted into residential accommodation. The atrium would be glazed and would incorporate louvres to provide natural ventilation. The atrium would not be visible from ground level.



Roof plan of the atrium



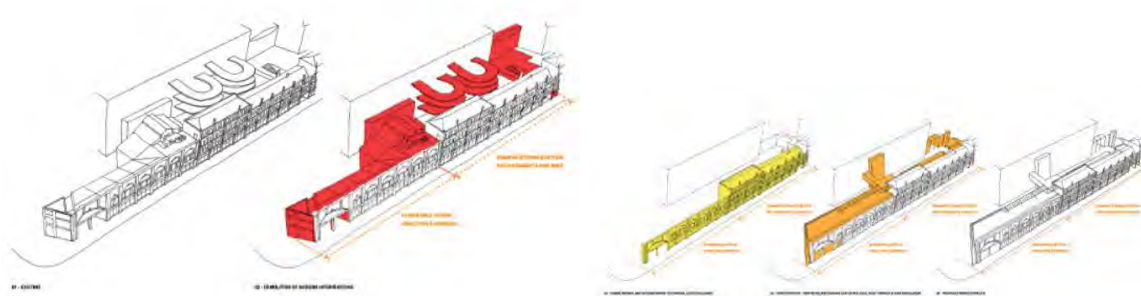
Image of the central atrium

The floorplates could be occupied by a single occupier or subdivided depending on potential tenants. The alterations and demolitions required to facilitate the conservation of the warehouse are considered elsewhere in this report. Externally appropriate masonry repairs would be undertaken. Internal walls would be left exposed to add character to the internal spaces. All the original metal windows would be repaired and re-glazed. Any new windows would match the original windows in terms of pattern and fenestration.

The works to the warehouse would be acceptable and would provide a sensitive alteration and refurbishment of the listed building. Whilst there would be an impact on listed fabric, which is considered elsewhere in this report, this would be relatively light touch and low impact. The window strategy and repair strategy would allow the building to be appreciated fully unlike the current car park use. The lift core and bridge would also be a high quality and simple addition to the building. The removal on unsympathetic additions would better reveal the significance of the building.

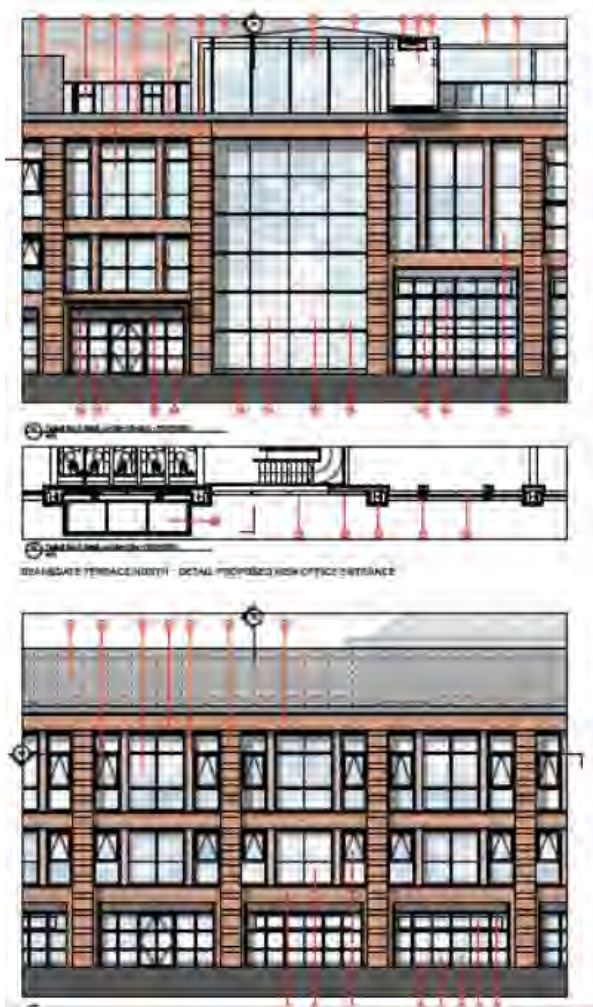
Deansgate Terrace North and Deansgate Terrace South would be refurbished in line with the SRF.

The non original structures added to the northern terrace in the 1990s would be removed. This includes the link building between the terrace and the warehouse. Masonry, rainwater foods and glazing would be repaired.



Demolition works in red (left) proposed development (right)

A new build and refurbishment is proposed in the northern section to create offices in the upper three levels and the commercial uses refurbished to activate Deansgate and Dean Street. A 3 storey extension would extend the full length of the northern section of the terrace. The extension would not exceed the parameters of the existing extension in terms of siting and scale. The gable end of the terrace would be chamfered to reflect the buildings original geometry. The design of the extension is contemporary interpretation of the historical façade with its vertical and horizontal rhythm and Crittal windows. A red brick would complement the warehouse and main façade of the terrace.



Extension to Deansgate Terrace North

New shop front glazing on the Deansgate façade to provide a new entrance.



Existing and proposed entrance to Deansgate (Deansgate Terrace North)

The gable elevation at the junction of Peter Street and Deansgate would have horizontal recesses in the brick façade. These would be inlaid with bronze strips with

perorated lettering inspired by 'The Masque of Anarchy' poem following the Peterloo massacre. The lettering would bit lit.



Refurbished Deansgate Terrace with its new entrance and gable

A roof terrace would be created to the new building element of the terrace and would be accessible by lift core and by the bridge link. This would provide outdoor recreational space and opportunities for planting. Rooflights would be created to provide natural light to the offices below.



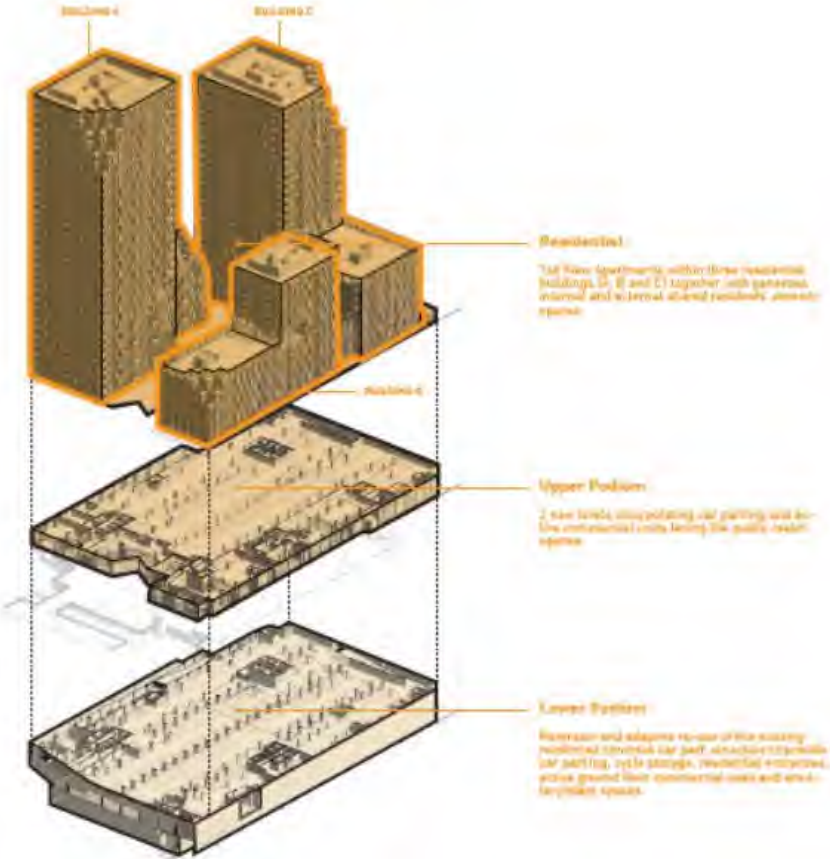
Roof terrace to Deansgate North

A signage and lighting strategy would be sympathetic to the building and would be secured by planning condition.

The works to Deansgate Terrace North would provide an opportunity to remove unsuitable additions to the building and replace them with a modern and high quality extension that would complement the historical façade. Final details would be agreed by planning condition.

The internal spaces of Deansgate Terrace South would have a light refurbishment.

The main elements of the new build would create homes. This would remove the upper section of the existing steel frame and adapt the lower podium to retain an element of car parking.



Composition of the Leisure Box development

The basement would be retained with new cores secured into the basement with plant. The ground floor would remain as a car park with refuse stores and cores. A retail unit and entrance lobby would be created on Watson Street. The first floor would contain parking spaces and the cycle store whilst the second floor would have two commercial units to animate the public realm on the Upper Deansgate Mews and viaduct. New parking would be created at the third floor.

The upper floors comprise of the homes. The development is split into 3 main towers – buildings A, B and C. Roof terraces would be provided on the 10th floor of building B and the 12th floor of buildings A and C.



Facades of the towers including roof terrace and podium amenity areas

Building A would be 34 storeys, building B 16 storeys and building C 27 storeys. The SRF states that the Leisure Box is suitable for a tall building. The towers would be above a podium with gaps between them. This reduces the overall impact of the built form and allows the lower portions to be used as roof terraces. The building corners are 'cut out' on one side which breaks massing down further.

Historic England have expressed concern about the scale of the new build describing that the towers would loom over the warehouse. They acknowledge that the impact of the towers would be partly diluted as they would be read as a group of tall buildings that exist in this area, but they don't consider that this negates their concern about the scale, including the proximity of the tallest element to the warehouse.

A robust analysis has informed the height of the proposal. Other configurations would cause a greater degree of harm to the setting of the listed warehouse.

The demolition of modern elements would better reveal all four facades for the listed warehouse for the first time in decades. This would be a significant heritage benefit of the scheme and would allow the legibility and significance of the building to be fully realised along with its sensitive repair and alteration.

Although there would be a tall building close to the warehouse, the elevational treatment would be high quality with cut outs, height variations and gaps between the towers. This reduces the overall impact the warehouse.

The impact of the height of the skyline has been tested through the townscape visual assessment which has determined that in most cases the development has a positive impact.

The appearance of the Leisure Box has been informed by the repetitive grid and simplicity of the Great Northern Warehouse and Deansgate Terrace. The main façade material would be a red pigmented pre cast concrete responding to the listed buildings. The upper and lower sections of the building would be treated as follows.

The outer face of the expressed precast concrete would have a polished finish to create a sheen across the buildings outer surface. The set back elements would have an expressed horizontal texture. The window would be positioned centrally within the grid openings and comprise a Crittal style window echoing the adjacent warehouse. The window openings would have full height glazing, solid insulated panels and decorative anodised perforated panels. Ventilation panels would be found in the soffit of the window.



Lower facades (left) and upper facade (right) elevation treatment

The precast grid continues the podium areas, ground and first floor areas. Deeper horizontal and vertical grid elements, and wider spacing of the grid, are proposed to provide a clear base to the building. These wider grid spacings are suitable for the glazed commercial frontages and residential entrances. These glazed elements would be Crittal style. Perforated screens would be provided to the car park areas. These would glow at night with the light from inside the building, providing illumination and animation when viewed externally.



TYPICAL LEVEL 1 DETAIL, SOUTH ELEVATION



TYPICAL WEST ELEVATION TO DEMONSTRATE VIEWS

Building base



East elevation (left) and North Elevation (right)



West elevation (left) and south elevation (right)

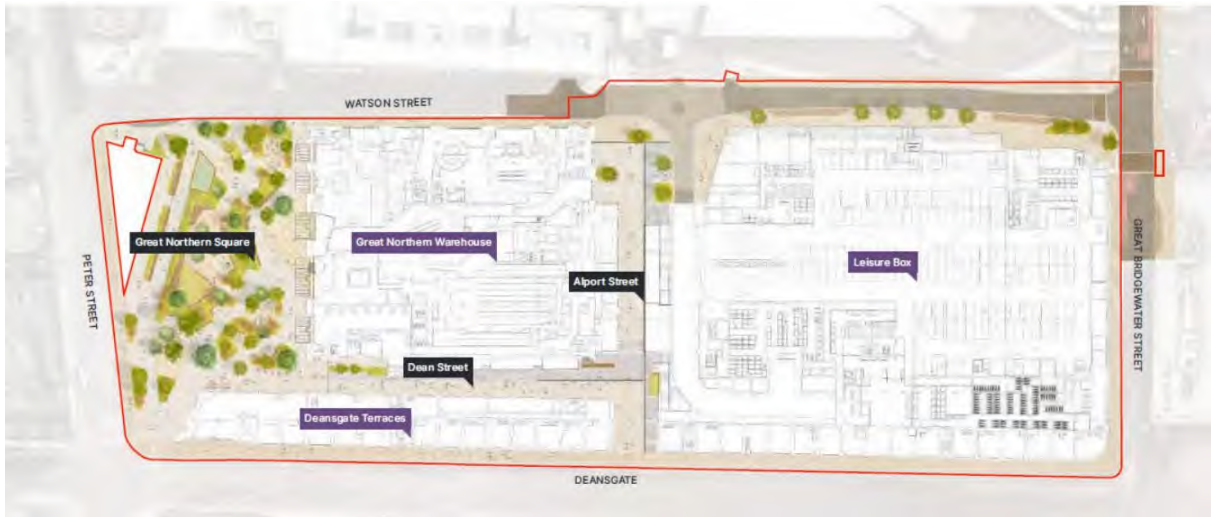
The design meets the standards required for a development of this scale in the City Centre. The tallest elements would be part of a cluster the other taller buildings in the area and away from the grade II* warehouse allowing its setting to be largely preserved. The architectural expression would be simple with high quality materials.

The proposals to the Great Northern Warehouse: would be sympathetic. Alterations to the atrium, external core and bridge would be minimised. The remaining works would result in a sensitive reuse of the building. The alterations to Deansgate Terrace north would be high quality and offer a more suitable addition to the buildings than the current additions.

Conditions would be used to ensure that the are acceptable to ensure the architecture is delivered to the required standard.

Contribution to Improving Permeability, Public Spaces and Facilities and Provision of a Well-Designed Environment

Green and accessible public realm would enhance the setting of the Grade II* and Grade II listed warehouse and Deansgate Terraces and improve connections, accessibility and permeability at the site. This would support the green and blue infrastructure strategy for the side as part of a sustainability strategy. This would equate to a 12,000 sqm of enhanced and new public realm and a £6 million investment in public realm.



Public realm masterplan

The public realm would consist of three main areas of work:

- The Square;
- Dean Street and Alport Street;
- Public realm first floor;
- Footway improvements;
- Building terraces; and
- Deansgate terrace roof garden.

The Square

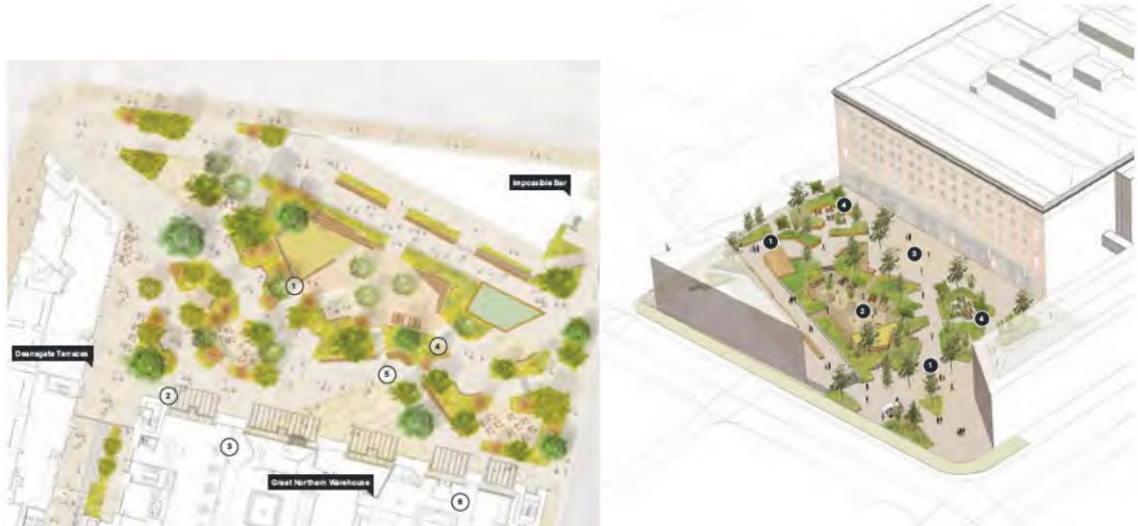
The square outside of the Great Northern Warehouse, with its current amphitheatre design, is not accessible due to the steps. There is a need to create clear desire lines around the warehouse and Dean Street to encourage movement.



The Square

The amphitheatre would be filled in and the access to the central space would be level. The steps around the Impossible Bar which are not part of this application would remain. However, connections are improved to the north and south sides of the square.

The Square would have a sand pit to create a family friendly space. Tiered seating would be created around the edges with a water feature, trees, informal play routes and picnic and bench seating.



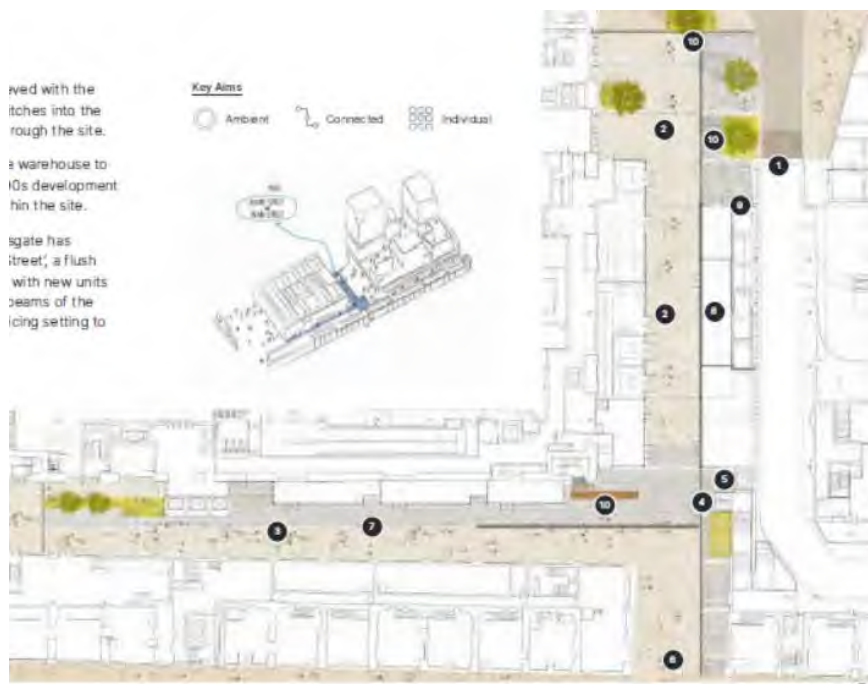
Public realm improvements to The Square

Seating would be provided at appropriate heights, with backrests and armrest in order to be accessible. There lighting would be enhanced to create a space that is safe in use the evening and darker months.

Bollards would provide security around the perimeter with wind mitigation features and art work. 56 cycle spaces would be provided.

Dean Street and Alport Street

The removal of modern additions from the warehouse would allow improved circulation and permeability around the warehouse and through the site. The removal of the stepped access from Deansgate would create an at grade route through to Watson Street known as Alport Street. New commercial units would be introduced under the viaduct revealing the original viaduct beams.



Dean Street and Alport Street

Public realm first floor

Deansgate Mews and the Viaduct space would be amended to create a through route. This new space would be occupied by vendors to enliven it.

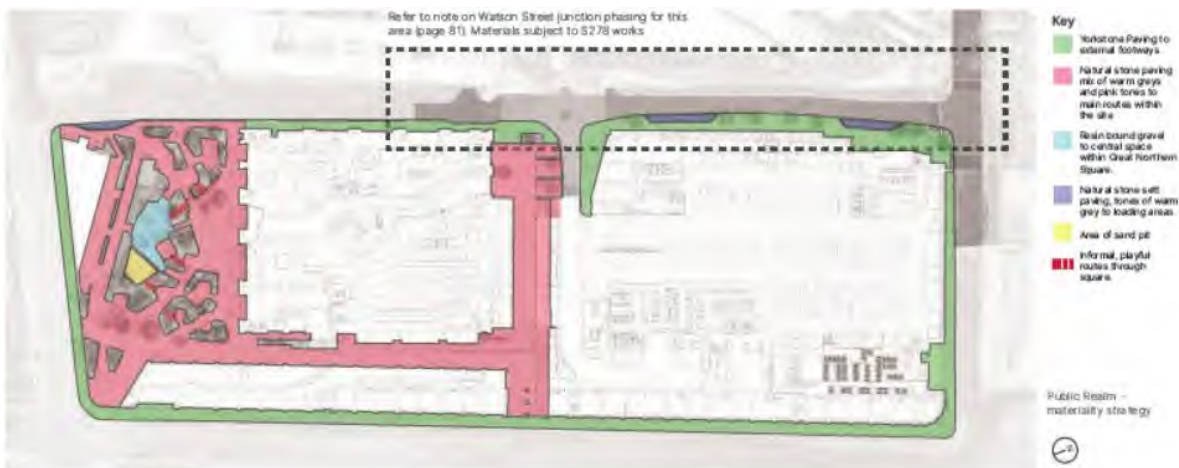


Public realm first floor

New steps would be created off Great Bridgewater Street to create a more generous approach to this part of the development. Within the street space, there would be fixed and moveable elements to maximise adaptability for future tenants.

Footway improvements

All the footways around the perimeter of the site would be improved with Yorkstone paving to respond to the historic environment. Natural stone paving would be used in the public realm in the site to contrast with the adopted highway.



Extent of footway improvements

Building Terraces

Resident terraces would be created in the residential tower. This includes a podium on the second floor which all residents would have access to and terraces for the individual towers (10 and 12th floors). They would be used for community uses.

In addition, a terrace is proposed for the office users within Deansgate Terrace with views over Great Northern Square.



Building terraces plan

The residents podium would have seating, planting and gym equipment. The podium would be a blue and green roof with water attenuation and biodiversity benefits.



Residents podium



Individual building terraces

Deansgate Roof Terrace

A roof garden on the top of Deansgate Terraces would provide a spill out space for office occupiers. It would have views across Great Northern Square. .



Deansgate Roof Terrace

Final details would be agreed as part of the planning conditions to ensure that the public realm is delivered to an appropriate standard with a landscape management plan for the future maintenance of the areas.

Impact on Trees

There are 6 tree groups on the site which have been subject to an arboricultural assessment and are classified as follows:

- Category A (High Value) – None
- Category B (Moderate Value) – 2 group trees

- Category C (Low Value) – 4 group trees

The proposal would result in the removal of 1 category B group and 4 category C trees.

Policy EN9 states that new developments should maintain green infrastructure. Where the benefits of a proposal are considered to outweigh the loss of an existing element of green infrastructure, the developer should demonstrate how this loss would be mitigated in terms of quantity, quality, function and future management.

The trees cannot be retained if the site is to be developed in a way which would deliver the significant regeneration benefits. 156 trees would be planted in the public realm and landscaping works in mitigation of those lost. This would bring biodiversity benefits which are considered elsewhere within this report. This would satisfy policy EN9 of the Core Strategy.

Impact on Ecology

An ecological appraisal concludes that the development would not cause significant or unduly harmful impacts to local ecology. It has been determined that the site and buildings have limited potential to support bats and bird provided that relevant precautions are put in place during demolition. The planting, trees and street trees would enhance green infrastructure, biodiversity and the ecological value of the site. A condition would agree final details to comply with policy EN9 of the Core Strategy and ensure a biodiversity gain at the site.

Effects on the Local Environment/ Amenity

- (a) Sunlight, daylight, overshadowing, glare and overlooking

Sunlight and daylight

An assessment has been undertaken to establish the likely effects on daylight and sun light received by properties around the site. Consideration has also been given to instances of overlooking which would result in a loss of privacy.

The BRE guidelines have been used to provide a method for assessing daylight – Vertical Sky Component (VSC), No Sky Line (NSL) and Average Daylight Factor (ADF) methods. For sunlight, the approach considers the Annual Probable Sunlight Hours (APSH) for a reference point on a window i.e. if a window point can receive at least 25% APSH, then the room should still receive enough sunlight.

The following properties were assessed as part of the survey:

- 303 Deansgate and Land at Great Bridgewater Street and Trafford Street (Beetham Tower);
- 321 Deansgate, Manchester;
- 332 & 334 Deansgate and 1A, 3, 7, 9 and 11 Liverpool Road;
- Porchfield Square and Rozel Square, Byrom Street, Manchester;

- Free Trade Hall, Peter Street, Manchester;
- Land on the East side of Watson Street, Manchester; and
- Viadux (Salboy Development).

Consideration should be given to paragraph 123 (c) of section 11 of the NPPF which states that when considering applications for housing, a flexible approach should be taken in terms of applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site; as long as the resulting scheme would provide acceptable living standards.

303 Deansgate and Land at Great Bridgewater Street and Trafford Street (Beetham Tower)

269 of the 270 windows (99%) to habitable rooms either exceed the BRE target or their VSC values do not reduce more than 20% which is acceptable within the guidelines. The reductions in VSC values to the remaining window is between 21% and 30% which is not considered to be unduly harmful in this city centre context. NSL values for all 126 rooms analysed (100%) do not reduce by more than 20% which is acceptable. There is no requirement to assess APSH as all 270 windows do not fall within 90 degrees of due south.

321 Deansgate, Manchester

All 17 windows (100%) to habitable rooms either exceed the BRE target figure or their VSC values do not reduce more than 20% which is acceptable within the guidelines. NSL values for all 5 rooms analysed (100%) do not reduce by more than 20% which is acceptable. There is no requirement to assess APSH as all 17 windows do not fall within 90 degrees of due south.

332 & 334 Deansgate and 1A, 3, 7, 9 and 11 Liverpool Road

All 14 windows to habitable rooms either exceed the BRE target figure or their VSC values do not reduce more than 20% which is acceptable. NSL values for all 7 rooms analysed do not reduce by more than 20% which is acceptable. There is no requirement to assess APSH in this context as all 14 windows do not fall within 90 degrees of due south.

Porchfield Square and Rozel Square, Byrom Street, Manchester

52 of 56 windows (93%) to habitable rooms either exceed the BRE target or their VSC values do not reduce more than 20% which would not be considered to be unduly harmful in a city centre context.

The reductions in VSC values to the remaining 4 windows (7%) are between 21% and 30%. All of these windows are to bedrooms rather than main habitable spaces. This is considered to be acceptable in this context and would not warrant refusal of the application.

NSL values for 42 of the 56 rooms analysed (75%) do not reduce by more than 20%. 11 of the remaining 14 rooms (20%) have a reduction in NSL between 21% and 30%

(less than 10% above the permitted 20% outlined in the guidelines). The 3 remaining bedrooms (5%) have a reduction in NSL between 31% and 40% (less than 20% above the permitted 20%). All these rooms are bedrooms which are not considered to be main habitable rooms and the overall effects on these spaces are not considered to be unduly harmful to warrant refusal in a city centre context.

56 windows were assessed for APSH with 36 (64%) being well above the BRE recommended levels of 25% in summer or they do not reduce by more than 20% as a result of the proposed development. 17 of the remaining 20 windows experience a reduction of between 21-20% with the remaining 3 windows reducing by between 31-40%.

The winter annual probable sunlight hours calculated that 32 of the 56 windows that were assessed (57%) were well above the BRE recommended levels of 5% in winter or do not reduce more than 20% as a result of the proposed development. 2 of the remaining 24 windows (4%) reduced by 31-40% within the remaining 22 windows (39%) reducing by more than 40%.

All the windows with a reduction in the summer and winter APSH serve bedrooms which are not main habitable rooms and given the city centre context, it is not considered that the effects are unduly harmful to warrant refusal.

Free Trade Hall, Peter Street, Manchester

All 80 windows (100%) to habitable rooms either exceed the BRE target or their VSC values do not reduce more than 20%. NSL values for all 70 rooms analysed (100%) do not reduce by more than 20% which is acceptable in a city centre context.

80 windows were assessed for summer and winter APSH. In summer, the windows are well above the BRE recommended levels or do not reduce by more than 20%. In winter, the windows again remain well above the BRE recommended levels of 5% or do not reduce by more than 20%. The impacts on APSH levels are acceptable.

Land on the East side of Watson Street, Manchester

254 of the 288 windows (88%) to habitable rooms exceed the BRE target or their VSC values do not reduce more than 20%. The reductions in VSC values to the remaining 34 windows (12%) are between 21% and 30% (less than 10% above the permitted 20%). 2 of these windows are bedrooms which are not considered to be main habitable rooms in the guidance. Whilst this would result in a minor impact it is not considered to be unduly harmful in this city centre context.

NSL values for 152 of the 159 rooms analysed (96%) do not reduce by more than 20%. 1 of the remaining 7 rooms reduced between 21-30% (less than 10% above the permitted 20%), 6 rooms reduced between 31-40% and 1 room by more than 40%. Overall the level of impact on this property is low and whilst there is a small amount of harm to a small number of windows, this is not considered to be of magnitude that would warrant refusal.

45 windows of 288 are assessed for APSH. These windows met the summer and winter annual sunlight hours being well above the BRE recommended levels of 25% in summer and 5% in winter or do not reduce by more than 20%.

Viadux (Salboy Development) (under construction)

701 of the 760 windows (92%) to habitable rooms exceed the BRE target or do not reduce by more than 20%. The reductions in VSC values to 48 of the remaining 57 windows (6%) are between 21% and 30% (less than 10% above the permitted 20%). The 11 remaining windows (2%) are between 31% and 40%. Whilst this would result in a minor impact it is not considered to be unduly harmful in this city centre context.

NSL values for 556 of the 570 rooms (98%) did not reduce by more than 20%. 2 of the rooms experience a reduction of between 21-30% and 3 experienced a reduction between 31-40%. The remaining 9 rooms experienced a reduction greater than 40%. It should be noted that all these rooms were bedrooms which are not considered as sensitive as main habitable rooms. Within a city centre context these impacts are considered to be acceptable and would not warrant refusal.

114 windows of 760 are required to be assessed for APSH. These windows met the summer and winter annual sunlight hours being well above the BRE recommended levels of 25% in summer and 5% in winter or do not reduce by more than 20%.

Overshadowing

The impact of the development on adjacent amenity areas (both permanent and transient overshadowing) has also been considered. The areas considered are as follows:

- Amenity area adjacent to the Great Northern;
- Amenity area adjacent to the Great Northern Tower;
- Amenity area Porchfield Square;

Adjacent Amenity Area (Great Northern)

This amenity area is located to the north of the proposal and forms a public space to the Great Northern building. This area would remain compliant with the BRE criteria regarding permanent and transient sunlight it receives.

Adjacent Amenity Areas (Great Northern Tower)

These amenity areas are to Northeast of the proposal and consist of balconies and roof top terraces to the Great North Tower building (B44). Many of these areas are already poorly sunlit due to their orientation and proximity to other upper parts of the building. The proposal would have a permanent impact on amenity areas at the lower levels of the sloping section of the building. 86 of the 112 areas (76%) would continue to received complaint levels of sunlight.

There would be some transient impact in the spring and summer but these would be for very short periods between 1 and 2 hours.

Given that these areas already receive limited sunlight, it is not considered that the impact overall would be unduly harmful to warrant refusal.

Adjacent Amenity Areas (Porchfield Square)

These amenity areas are located north west of the proposal and are garden areas associated with Porchfield Square. 3 of the 4 areas would continue to receive good levels of permanent sunlight. The remaining amenity area (known as A122) currently does not receive much sunlight and the development would have a minor impact on this further. There are no significant transient impacts, particularly in the June/summer months when shadows are shorter. It is considered that this impact is acceptable within a city centre context and would not be harmful enough to warrant refusal given the existing conditions.

Glare

A glare assessment has been prepared. A 3D computer model of the existing local area, roads, train track, train station and signals has been created to calculate the glare levels in 10 selected viewpoints. These are as follows:

- C1 – Traffic lights, Liverpool Road, facing development
- C2 – T-Junction, Tonman St, facing development
- C3 – Round-about Junction, Watson St, facing development
- C4 - Eastbound track, approaching signal
- C5 - Eastbound track, approaching signal
- C6 - Traffic lights, Whitworth St, facing development
- C7 – T-Junction, Camp St, facing development
- C8 - Traffic lights, Junction of Deansgate and Quay St, facing development
- C9 - Traffic lights, Great Bridgewater St, facing development
- C10 - Traffic lights, Junction Deansgate and Whitworth St, facing development

The study identified that there were three locations where additional glare could occur (C2, C5 and C9). In all other locations glare would not occur.

C2 T-junction of Tonman St and Deansgate

Some glare would occur between 7am to 9am between April to September and 3pm to 5pm April to August (90 degrees) and 7am to 9am March through to May, and August to September, also at 4pm to 5pm May through to August (30 degrees).

The location of this view, and the relatively minor occurrence of the glare, it is not considered that this would give rise to any unduly harmful impact or safety concerns that would warrant refusal..

C5 - Eastbound track, approaching signal

Some glare would occur at 90 degrees at this location at 5am between April and September, 9am in January, November and December and at 4pm to 6pm between January to March and September to November. There would also be a small amount of glare at 30 degrees during the same times and month.

The location of this view, and the relatively minor occurrence of the glare, it is not considered that this would give rise to any unduly harmful impact, safety concerns or conflict with rail signals that would warrant refusal of this application.

C9 - Traffic lights, Great Bridgewater St, facing development

Some glare would occur at 90 degree field in this location from 6am to 10am between February and October and 4pm to 7pm between April through August. There would also be a small amount of glare at 30 degrees between 7am and 9am in March to April and August to October and at very small angular amount at 6pm May to July.

It is noted that the glare in this location would affect one of the traffic lights along Great Bridgewater Street. This glare would, however, occur at the top of the field of vision and therefore can be shielded by a sun visor for those in a vehicle. In addition, there are alternative traffic lights to the left hand side of the lane and across the junction which are outside of the field of glare.

The location of this view, and the relatively minor occurrence of the glare, it is not considered that this would give rise to any unduly harmful impact, safety concerns or conflict with traffic signals that would warrant refusal.

Overlooking

The proposal is separated from existing developments by the road network and the distances between the surrounding developments are considered to be acceptable.

The oblique/offset relationship between the proposal and the Great Northern Tower means that there would be no direct overlooking to the new buildings. The distance between the two buildings at the closest point (measured corner to corner) is 42m. The relationship between the proposal and the Beetham Tower is also offset, avoiding direct overlooking across the main faces of the buildings. The separation distance is approximately 27.5m, measured to the face of the cantilevered upper residential floors of the Beetham Tower. The height of the proposal relative to the height of the apartments in the Beetham Tower, which are situated at level 25 and above, means that views from the Beetham Tower apartments would mostly be above the height of the nearest residential building on the Great Northern Site.



Privacy distances and views from adjacent developments

(b) TV reception

A TV reception survey has concluded that there is unlikely to be any interference with digital terrestrial and satellite television. This would be closely monitored during the works and a condition would require of a post completion survey to be undertaken to verify any impacts and secure mitigation if required.

(c) Air Quality

The site is in the Greater Manchester Air Quality Management Air (AQMA) where air quality conditions are poor. Roads which may be used for construction traffic and post development are in the AQMA. The site is close to homes, educational establishments, offices, hotel, medical facilities and other commercial uses.

These uses could be affected by construction traffic and that associated with the completed development and have been identified as having a high to medium sensitivity to local air quality conditions.

The application assesses the potential effects during construction of dust and particulate emissions from site activities and materials movement based on a qualitative risk assessment method based on the Institute of Air Quality Management's (IAQM) 'Guidance on the Assessment of Dust from Demolition and Construction' document, published in 2014.

The assessment of the air quality impacts of the completed development has focused on the predicted impact of changes in ambient nitrogen dioxide (NO₂) and particulate matter with an aerodynamic diameter of less than 10 µm (PM₁₀) and less than 2.5 µm (PM_{2.5}) at key local locations. The magnitude and significance of the changes

have been referenced to non-statutory guidance issued by the IAQM and Environmental Protection UK (EPUK).

Both the construction and operational impacts of the development on air quality have been considered.

The main contributors to air quality conditions would be from construction. dust, particulate matter and pollution concentrations generated on site, particularly from exhaust emissions from traffic, plant and earthworks. Nearby homes are likely to experience impacts from dust from construction and earthworks. The air quality report identified that residential properties along Tonman Street, Camp Street, St John Street and Artillery Street and immediately east of the Site on Watson Street – in particular Great Northern Tower which is a residential building are most likely to be affected. There are also likely to be cumulative impacts from other nearby developments which will be under construction at the same time.

The impact on human health would be medium for demolition works and low for earthworks, construction and trackout activities. The main impact on local air quality conditions is from construction traffic along nearby roads. Construction traffic would enter the site via Deansgate, Great Bridgewater Street and Watson Street, however, the volume of traffic is expected to be low in comparison to existing traffic flows.

With appropriate mitigation in place, such as dust suppression measures, no idling of vehicles, avoidance of diesel or petrol powered plant, speed restrictions on unpaved roads, and the implementation of a Construction Logistics Plan and Travel Plan, the impact on local air quality conditions should be minimised. These measures would be secured through the construction management plan condition.

Consideration has been given to the impact on quality conditions on future occupants of the development and the surrounding area when the development is occupied.

Although the development would generate traffic, it would not create new impacts on air quality conditions (NO₂, PM₁₀ and PM_{2.5}).

The number of parking spaces would be reduced to 929, a net loss of 311 spaces. 20% of the remaining spaces would be fitted with an electric vehicle charging point. There would be 100% cycle spaces and a travel plan to encourage the use of public transport use and reduce vehicle trips.

As the development would operate on an all electrical system, there would be no gas fired boilers or generators which would normally contribute to air quality conditions. No mitigation is required to minimise the impact when the development is occupied. A mechanical ventilation system would ensure that air intake to the homes would be fresh and free from pollutants.

Environmental Health concur with the conclusions and recommendations within the air quality report. The mitigation measures would be secured by planning condition and the proposal would comply with policy EN16 of the Core Strategy, paragraph 8 of the PPG and paragraph 124 of the NPPF in that there would be no detrimental impact on existing air quality conditions as a result of the development.

(d) Wind environment

A wind assessment has examined potential effects and in particular, wind flows that would be experienced by pedestrians and the influence on their activities. The assessment considered mitigation measures to minimise these impacts.

A Computational Fluid Dynamics (CFD) analysis assessed the effects of the proposal on existing wind conditions, the conditions with the development in place and the cumulative scenario with other committed developments. Scenarios (including existing conditions) have been modelled to determine the wind speeds at the site and the impact on pedestrian comfort and safety.

The current wind conditions, for pedestrian safety and comfort, shows that most locations are safe for all users. There are two locations where pedestrian safety is currently exceeded at the corner of Beetham Tower on Great Bridgewater Street and the junction of Watson Street and Windmill Street. There is one location where pedestrian comfort is currently exceeded at the entrance of Albert Schloss.

The proposal would have a minimal impact on pedestrian safety with conditions predicted to be in line with current conditions. Conditions improve Watson Street/Windmill Street when considered cumulatively with other development.

Pedestrian comfort also remains broadly in line with current conditions with all areas remaining suitable for intended uses including balconies for sitting in the summer and podium terraces for standing and sitting.

In order to maximise the use of external areas, measures have been incorporated to minimise wind conditions in and around the site. These include fins and screens to the south east corner of the development together with screens along Watson Street. Provision of street trees within the landscaping scheme.

Mixture of solid and porous balustrades to the western facing balconies, porous balustrades to the north and south facing balconies and all other private balconies.

Noise and vibration

A noise assessment identifies the main sources during construction would be from plant, equipment and general construction activities, including breaking ground and servicing. Noise levels from construction would be acceptable provided the strict operating and delivery hours are adhered to along with the provision of an acoustic site hoarding, equipment silencers and regular communication with residents. This should be secured by a condition.

When the development is occupied, the acoustic specification of the homes would limit noise ingress from external noise, particularly nearby roads. This would be verified prior to occupation. Acoustic insulation would be required to the commercial and workspace accommodation to prevent unacceptable noise transfer.

Provided that construction activities are carefully controlled and the plant equipment and apartments are appropriately insulated the proposal would be in accordance with policy DM1 of the Core Strategy, extant policy DC26 of the UDP and the NPPF.

Waste management

The residential and commercial waste requirements have been carefully considered for this complex development.

Each apartment would have storage for refuse, recyclable and compostable materials in the kitchen and utility area. Separate compartments would be provided for each waste stream. Residents would be responsible for taking waste to the waste chutes located close to each of the cores. The total amount of waste storage across the 3 stores would be in line with the City Council waste guidance: 395 sqm is proposed (321 sqm is required by the guidance).

50% of the bins in the waste stores would be dedicated to recycling, combining mixed recycling, glass and organic (food). The management company would be in charge of monitoring the recycling rates and promote actively high recycling rates.

The collection point for all uses is located in the service area at the Leisure Box, in close proximity to the proposed waste stores. Access to this area will be controlled via a barrier system.

Environmental Health consider the waste arrangements to be acceptable and in line with City Council waste guidance for high rise residential developments.

Accessibility

All main entrances would be level. The residential entrances avoid pinch points with a low level reception desk and other measures to help wheel chair users. All upper floors are accessible by lifts and internal corridors would be a minimum of 1500mm. All apartments have been designed to space standards with adequate circulation space. The applicant has demonstrated that all apartments (1, 2 and 3 bedroom accommodation) can be adapted to be fully accessible (M4 (2) standard). These would incorporate a level access shower and appropriate turning area. There would be 5% dedicated parking space for disabled people created within the car parks.

Flood Risk/surface drainage

The site is in flood zone 1 'low probability of flooding' and in a critical drainage area where there are complex surface water flooding problems from ordinary watercourses, culverts and flooding from the sewer network. These areas are particularly sensitive to an increase in surface water run off and/or volume from new developments which may exasperate local flooding problems. Policy EN14 requires development to minimise its impact on surface water run off in critical drainage area.

A drainage statement has been considered by the City Council's flood risk management team. Further details are required to complete the drainage strategy in

order to satisfy the provision of policy EN14 of the Core Strategy which should form part of the conditions of the planning approval.

Impact on the highway network/car/cycle parking and servicing

A transport statement notes that all sustainable transport modes are nearby. The car park would be reduced by 311 spaces to 929 on the Leisure Box site. 167 (18%) would be allocated for the residential. 5% of the parking would be for disabled residents together with visitor disabled parking. 10% of the spaces would be fitted with an electric car charging point with 10% fitted with the infrastructure so they can be adapted at a later stage. A car club bay would also be provided in the car park.

100% cycle provision is proposed. A travel plan would support the travel needs of residents including whether any offsite parking is required. A condition should ensure that the travel plan is monitored. Servicing would take place from Watson Street where improvement would be made to the loading bays.

The proposal would make significant improvements the public realm and highway network around the application site. This includes preventing a through route along Great Bridgewater Street and improved public realm along Watson Street.

Additional modelling would be required to understand how vehicle trips would be displayed across the network and to help inform the highway improvement works.

undertaken demonstrates that the vehicle movements associated with development can be accommodated on the highway network.

The proposals are considered to be acceptable and would not have a detrimental impact on highway or pedestrian safety. Alterations would be made to the surrounding road network to ensure that the junctions and crossings are safe and improvements to cycle and pedestrians facilities would be made. The proposal therefore accords with policies SP1, T1, T2 and DM1 of the Core Strategy.

Designing out crime

A Crime Impact Statement (CIS), prepared by Design for Security at Greater Manchester Police, recognises that the development would bring vitality to this area and more active frontage. A condition is recommended requiring the CIS to be implemented in full to achieve Secured by Design Accreditation.

Ground conditions

A ground conditions report details that the site is contaminated from previous uses and requires remediation prior to redevelopment. The ground conditions are not complex so as to prevent development provided a strategy is prepared, implemented and the works verified. This approach should form a condition of the planning approval in order to comply with policy EN18 of the Core Strategy.

Construction management

The construction programme would last for approximately 5 years within which there would be elements of ground works and utility diversions, foundations, frame construction, façade cladding and internal fit out. There would be two main construction phases to the development

The construction programme would also need to minimise the impact on existing tenants that would remain on site (such as the Casino, bowling alley and food and beverage outlets) together with the effects on residential buildings and businesses in the local area.

The construction activity would fall into two main areas:

- Phase A - Warehouse conversion (including demolition of glazed atrium link) and Deansgate Terrace North plus Great Northern Square; and
- Phase B and C - (potentially with some overlap of Phase A) partial demolition of the Leisure Box, delivery of the towers and conversion of the southern end of Deansgate Terrace.

Phase A

Enabling works are estimated to last for 6 months. The main construction works would then begin in June 2024 with works on the Great Northern Warehouse commencing in August 2024. Works to the warehouse are expected to last for 21 months until March 2025. Works would then commence on the Northern Terraces in March 2025 and would last for 21 months until March 2026. Phase A works would also include the northern public realm works. On completion of the phase A works, full public access would be made available whilst construction works to the Leisure Box are ongoing with shared access to the central service area.

The site would be secured by a solid hoarding. Initial access and egress would be via the vehicle entrance on Watson Street until works have been completed to allow a one way system. Once the one way system has been implemented, access would be taken from Peters Street and Great Bridgewater Street with egress then through the remodelled Watson Street. Access and egress to the existing car park within the Leisure Box would be reconfigured on Watson Street to allow continued access to the car parking spaces.

The majority of the public realm to the north of the warehouse would remain accessible during the phase A works subject to localised works. Where possible vehicle off loading points will be located within the site boundary, where this is not possible crane off load points will be located on the highway in approved loading bay positions on Watson Street and Deansgate.

Phase B and C

The existing car park would be reduced to 100 spaces at ground and basement levels and would remain in occupation throughout the construction period. Enabling

works would take approximately 6 months being completed in June 2024. Strip out would take place in April 2024 and demolition works would commence in May. The main construction works would commence in June 2024. Construction is estimated to take 58 months and would be completed in June 2029. Works to the facade would start in December 2027 and last for 14 months.

Towards the end of the Phase A construction works, the site entrance off of Peters Street will be removed to allow for completion of hard landscaping works to the Great Northern Square.

The one way site access and egress system will then operate with a single access into site from Great Bridgewater Street with egress from site through the remodelled existing entrance on Watson Street. Servicing to the newly completed Phase A area will be via the central area under banksmen control. Where possible vehicle off loading points will be located within the site boundary, where this is not possible crane off load points will be located on the highway in approved loading bay positions on Watson Street and Deansgate.

An initial traffic management plan indicates that all HGV traffic approaching from the west would turn right onto Deansgate from Quay Street or join the site to the north access off Peter Street. HGVs would then egress via Watson Street, Deansgate and Great Bridgewater Street to the south boundary carriageway of the A56.

Dust mitigation measures would be employed in the interest of air quality and plant and equipment would be fitted with silencers and would take place during working hours only. Construction waste management would be in place at all times.

The work would take place close to homes and comings and goings are likely to be noticeable. However, these impacts should be only associated with the length of the construction, are predictable and can be mitigated against. A condition requires a construction management plan to be agreed which would include details of dust suppression measures, highways management plan and details of use of machinery. Wheel washing would prevent any dirt and debris along the road and beyond.

Provided the initiatives outlined above are adhered to, it is considered that the construction activities are in accordance with policies SP1 and DM1 of the Core Strategy and extant policy DC26 of the Unitary Development Plan. However, it is recommended that a condition requires the final construction management plan to be agreed in order to ensuring the process has the minimal impact on surrounding residents and the highway network.

Aerodrome Safeguarding

There are no aerodrome safeguarding concerns with regards to this proposal subject to an informative about the use of cranes during construction.

Fire Safety

It is a mandatory planning requirement to consider fire safety for high rise buildings in relation to land use planning issues. A fire statement must be provided, and the

Health and Safety Executive (HSE) must be consulted. Government advice is very clear that the review of fire safety at gateway one through the planning process should not duplicate matters that should be considered through building control.

A number of queries raised by the HSE have been addressed during the course of the application.

It is recommended that an informative of the planning approval highlights the need for further dialogue with relevant experts as part of the approval of Building Regulations in order to ensure that all matters relating to fire safety meet the relevant Regulations.

Permitted Development

The National Planning Policy Guidance states that only in exceptional circumstances should conditions be imposed which restrict permitted development rights otherwise such conditions are deemed to be unreasonable.

It is recommended that the permitted development rights that would normally allow the change of use of a property to a HMO falling within use classes C3(b) and C3(c) be restricted and that a condition be attached to this effect. This is important given the emphasis and need for family housing in the city.

It is also considered appropriate to remove the right to extend the new building apartment building upwards and remove boundary treatments without express planning permission as these would, it is envisaged, could undermine the design quality of the scheme and in respect of boundary treatment, remove important and high quality features from the street scene.

Legal Agreement

A legal agreement under section 106 of the Planning Act would secure a mechanism to re-test the viability of the scheme at an agreed future date to determine if there has been a change in conditions which would enable an affordable housing contribution to be secured in line with policy H8 of the Core Strategy as explained in the paragraph with heading "Affordable housing".

Conclusion

The proposal conforms to the development plan taken as a whole as directed by section 38 (6) of the Planning and Compulsory Purchase Act 2004 and there are no material considerations which would indicate otherwise.

This is in an important site in the Great Northern, Manchester Central and Castlefield Quay SRF which is suitable for a high density development with the opportunities to repurpose the listed buildings. There is an extant planning permission for reuse of the warehouse and the creation of new residential apartments.

This mixed-use development would result in significant heritage benefits through the refurbishment of the Grade II* warehouse for 26,000 sqm of Grade A office space,

refurbishment of Deansgate Terrace for commercial and office space and the creation of 746 homes which would contribute positively to housing supply in the City and population growth. £6 million would be invested in public realm.

One, two and bedroom homes would be created with ancillary amenity spaces, residents lounges, gym and active ground floor commercial uses. The residential tower would have a distinctive architecture which would complement the historic environment and the City's skyline.

The buildings would be of a high standard of sustainability. The buildings would be energy efficient and operate on an all electric system offering the most suitable long terms solution to energy supply at the site and carbon reductions. There would be a green and blue roof to the residential podium together with photovoltaic and air source heat pumps to the listed buildings.

The constraints of refurbishing the listed buildings and the overall ability to deliver the proposal to the standard outlined in this application, would result in the development not being viable if an affordable housing contribution was to be secured. A review of the viability at a later stage would be secured to test this at a future date. The scheme also secures significant improvement in public realm including highway improvements at Great Bridgewater Street/Watson Street to reduce traffic flows in this part of the city centre in order to create a more pedestrian friendly environment.

Careful consideration has been given to the impact of the development on the local area (including residential properties, business, rail, road and recreational areas) and it has been demonstrated that there would be no unduly harmful impacts on noise, traffic generation, air quality, water management, wind, solar glare, contamination or loss of daylight and sunlight. Where harm does arise, it can be appropriately mitigated, and would not amount to a reason to refuse this planning application.

The buildings and its facilities are fully accessible to all user groups. The waste can be managed and recycled in line with the waste hierarchy. Construction impacts can also be mitigated to minimise the effect on the local residents and businesses.

There would be some localised impacts on the conservation area and listed buildings with the level of harm being considered low, less than substantial and significantly outweighed by the substantial public benefits which would be delivered as a consequence of the development socially, economically and environmentally: S66 of the Listed Buildings Act (paragraphs 193 and 196 of the NPPF).

Other Legislative Requirements

Equality Act 2010

Section 149 (Public Sector Equality Duty) of the Equality Act 2010 requires due regard to the need to: Eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by the Act and; Advance equality of opportunity between persons who share a protected characteristic and persons who do not share it. The Equality Duty does not impose a legal requirement to conduct an Equality Impact Assessment. Compliance with the Equality Duty involves consciously thinking about the aims of the Equality Duty as part of the process of decision-making.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved policies of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the of the application is proportionate to the wider benefits of and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation **Minded to Approve subject to the signing of a section 106 agreement in relation a future review of the affordable housing position**

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. Pre application advice has been sought in respect of this matter where early discussions took place regarding the siting/layout, scale, design and appearance of the development and impact heritage assets. Further work and discussion have taken place with the applicant through the course of the application. The proposal is considered to be acceptable and therefore determined within a timely manner.

Conditions of approval for planning application 135565/FO/2022

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2) The development hereby approved shall be carried out in accordance with the following drawings and documents:

Drawings

10422-SHP-Z0-A-BD59-Z460-PL-XX-001, 10422-SHP-Z0-A-B5D9-Z460-PL-XX-002, 10422-SHP-Z0-A-B5D9-G100-PL-00-001, 10422-SHP-Z0-A-B5D9-G100-PL-01-001,

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SHP-Z4-A-B5D9-F200-PL-XX-003, 10422-SHP-Z4-A-B5D9-F200-PL-XX-004 and 10422-SHP-Z4-A-B5D9-F200-PL-XX-005

The above documents were received by the City Council, as Local Planning Authority, on the 24 November 2022

Supporting information

Design & Access Statement prepared by SimpsonHaugh, Public Realm Strategy prepared by Planit.IE, Façade Lighting Strategy prepared by Light Bureau, Stage 2 Civil and Structural Report prepared by Curtins, Condition Surveys prepared by Aura and Quadriga, Statement of Community Involvement prepared by Forever Consulting, Archaeological Desk Based Assessment and covering note prepared by Salford Archaeology, Environmental Standards Statement including BREEAM Pre-Assessment prepared by WSP, Ventilation, Extraction and Odour Statement prepared by WSP, Energy Statement prepared by WSP, Preliminary Ecological Assessment and Biodiversity Net Gain Assessment prepared by Indigo Surveys, Arboricultural Impact Assessment prepared by Indigo Surveys, Crime Impact Statement prepared by GMP, Threat and Risk Security Needs Assessment prepared by Grendon, Transport Assessment prepared by Curtins, Travel Plan Framework prepared by Curtins, Waste Management Strategy prepared by Curtins, Television Baseline Survey Report (pre-development) prepared by Pager Power, Aviation Impact Assessment prepared by Pager Power, Telecommunications Impact Assessment prepared by Pager Power, Viability Assessment prepared by CBRE, Place Strategy including Local Benefits Proposal prepared by Trilogy, Fire Statement prepared by OFR Consultants, Flood Risk Assessment and Detailed Drainage Strategy (Great Northern Warehouse and Deansgate Terrace) prepared by Curtins, Flood Risk Assessment and Drainage Strategy (Leisure Box) prepared by Civic Engineers, UXO Risk Assessment prepared by 1st Line Defence and Phase 1 Preliminary Risk Assessment prepared by Curtins

The above documents were received by the City Council, as Local Planning Authority, on the 24 November 2022

Environmental Impact Assessment

Chapters 1-4: Introductory Chapters:
Appendix 2.1. Scoping Report Request
Appendix 2.2. Scoping Opinion from LPA

Chapter 5 Construction Management
Appendix 5.1 Construction management, Demolition and Logistics Plan prepared by Real PM

Chapter 6: Townscape and Visual Impact Assessment
Appendix 6.1: Zone of Visual Influence and Viewpoints Plan
Appendix 6.2: Baseline Photography
Appendix 6.3: Proposed TVIA Views

Chapter 7: Historic Environment
Appendix 7.1: Heritage Statement

Chapter 8: Air Quality

Appendix 8.1: Glossary of Terms

Appendix 8.2: Legislation, Policy and Guidance

Appendix 8.3: Windrose

Appendix 8.4: Traffic Data

Appendix 8.5: Receptors and Background Concentrations

Appendix 8.6: Verification Details

Appendix 8.7: Impact Assessment Results

Chapter 9: Noise and Vibration

Appendix 9.1: Legislation & Policy

Appendix 9.2: Baseline Noise Survey Details

Appendix 9.3: Construction Noise Assessment

Appendix 9.4: Overheating & Noise Assessment

Chapter 10: Wind

Appendix 10.1: Wind Microclimate Assessment

Chapter 11: Sunlight, Daylight, Overshadowing & Solar Glare

Appendix 11.1: Daylight & Sunlight Report;

Appendix 11.2: Overshadowing Report

Appendix 11.3: Glare Report

Chapter 12: Socio-Economics

Chapter 13: Human Health

Chapter 14: Climate Change

Chapter 15: Type 1 Cumulative Effects; and

Chapter 16: Summary of Residual Effects.

EIA Non-Technical Summary

The above documents were received by the City Council, as Local Planning Authority, on the 24 November 2022

Agent response to highway comments dated 20 January 2023

Ecology Appraisal received by the City Council, as Local Planning Authority, on the 17 January 2023

Fire Service response, response to highways, response to HSE including Leisure Box outlines for strategy & for service access (residential), response to TfGM/Highways. Update to Appendix 8 received by the City Council, as Local Planning Authority, on the 13 January 2023

Structural report received by the City Council, as Local Planning Authority, on the 15 December 2022

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy.

3) Unless all phases are commenced concurrently, the phasing of the development shall be carried out in accordance with drawing 10422-SHP-Z0-A-B5D9-Z460-PL-XX-001 stamped as received by the City Council, as Local Planning Authority, on the 24 November 2022 and shall be herein have the following meanings:

Phase A – refurbishment and conversion of the Great Northern Warehouse including removal of modern features and the refurbishment of the northern section of Deansgate Terrace and enhancement of Great Northern Square

Phase B and C – partial demolition of the Leisure Box and retention of car parking and the erection of the residential element (B) and refurbishment and conversion of the southern end of Deansgate Terrace (C).

Reason – If the development is to be carried out on a phased basis then this condition details the sequence of that phasing pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

4) No demolition works or vegetation clearance shall take place during the optimum period for bird nesting (March - September inclusive) unless nesting birds have been shown to be absent, or, a method statement for the demolition including for the protection of any nesting birds is agreed in writing by the City Council, Local Planning Authority. Any method statement shall then be implemented for the duration of the demolition works.

Reason - In order to protect wildlife from works that may impact on their habitats pursuant to policy EN15 of the Manchester Core Strategy (2012).

5) In this condition "retained tree" means an existing tree, shrub or hedge which is to be as shown as retained within the Arboricultural Impact Assessment prepared by Indigo Surveys stamped as received by the City Council, as Local Planning Authority, on the 24 November 2022; and paragraphs (a) and (b) below shall have effect until the expiration of 5 years from the date of the occupation of the building for its permitted use.

(a) No retained tree shall be cut down, uprooted or destroyed, nor shall any retained tree be topped or lopped other than in accordance with the approved plans and particulars, without the written approval of the local planning authority. Any topping or lopping approved shall be carried out in accordance with British Standard 5387 (Trees in relation to construction)

(b) If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the local planning authority.

(c) The erection of fencing for the protection of any retained tree shall be undertaken in accordance with the approved plans and particulars before any equipment, machinery or materials are brought on to the site for the purposes of the development, and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Nothing shall be stored or placed in any area fenced in accordance with this condition and the ground levels within those areas shall not be altered, nor shall any excavation be made, without the written consent of the local planning authority.

Reason - In order avoid damage to trees/shrubs adjacent to and within the site which are of important amenity value to the area and in order to protect the character of the area, in accordance with policies EN9 and EN15 of the Core Strategy.

6) All tree work should be carried out by a competent contractor in accordance with British Standard BS 3998 "Recommendations for Tree Work".

Reason - In order avoid damage to trees/shrubs adjacent to and within the site which are of important amenity value to the area and in order to protect the character of the area, in accordance with policies EN9 and EN15 of the Core Strategy.

7) Notwithstanding the details submitted on the Flood Risk Assessment and Detailed Drainage Strategy (Great Northern Warehouse and Deansgate Terrace) prepared by Curtins and the flood risk assessment and drainage strategy (Leisure Box) prepared by Civic Engineers, received by the City Council, as Local Planning Authority, on the 24 November 2022, (a) a phase of the development shall not commence until a scheme for the drainage of surface water from the development for that phase shall be submitted for approval in writing by the City Council as the Local Planning Authority. This shall include:

- A finalised drainage layout showing all components, outfalls, levels and connectivity;
- Maximised integration of green SuDS components (utilising infiltration or attenuation) if practicable;
- An existing and proposed impermeable areas drawing to accompany all discharge rate calculations;
- Details of surface water attenuation that offers a reduction in surface water runoff rate in line with the Manchester Trafford and Salford Strategic Flood Risk Assessment, i.e. at least a 50% reduction in runoff rate compared to the existing rates with the aim of reducing to the Greenfield runoff rates, as the site is located within Conurbation Core Critical Drainage Area;
- Great Northern warehouse - restricted discharge rate of 47 l/s;
- Great Northern Leisure Box - restricted discharge rate of 31.55 l/s;
- Further survey works required to confirm canal tunnel does not affect proposed drainage;
- Runoff volume in the 1 in 100 year, 6 hours rainfall shall be constrained to a value as close as is reasonable practicable to the greenfield runoff volume for the same event, but never to exceed the runoff volume from the development site prior to redevelopment;
- The proposed flood risk assessment and drainage strategy should include a description of the "Manchester & Salford Junction Canal Tunnel" asset which is indicated to be beneath the development, but its modern-day functionality is

- unspecified. This should also identify the asset owner, detail any consultation required, and any embedded design required to protect the asset;
- Evidence that the drainage system has been designed (unless an area is designated to hold and/or convey water as part of the design) so that flooding does not occur during a 1 in 100 year rainfall event with allowance for 45% climate change;
 - Assessment of overland flow routes for extreme events. Overland flow routes need to be designed to convey the flood water in a safe manner in the event of a blockage or exceedance of the proposed drainage system capacity including inlet structures. A layout with overland flow routes needs to be presented with appreciation of these overland flow routes with regards to the properties on site and adjacent properties off site;
 - Progression through the drainage hierarchy shall be evidence based and supported by site investigation. Results of ground investigation carried out under Building Research Establishment Digest 365. Site investigations should be undertaken in locations and at proposed depths of the proposed infiltration devices. Proposal of the attenuation that is achieving half emptying time within 24 hours. If no ground investigations are possible or infiltration is not feasible on site, evidence of alternative surface water disposal routes (as follows) is required;
 - Where surface water is connected to the public sewer, agreement in principle from United Utilities is required that there is adequate spare capacity in the existing system taking future development requirements into account. An email of acceptance of proposed flows and/or new connection will suffice;
 - For sites where proposed development would cause pollution risk to surface water, evidence of pollution control measures (preferably through SuDS) is required;
 - Hydraulic calculation of the proposed drainage system;
 - Construction details of flow control and SuDS elements; and
 - A feasibility study and details of the Green / Blue Roof

(b) Each phase of the development shall then be constructed in accordance with the approved details, within an agreed timescale.

Reason - To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution pursuant to policies SP1, EN14 and DM1 of the Manchester Core Strategy (2012).

8) a) Notwithstanding the Phase 1 Preliminary Risk Assessment, Great Northern, Curtins, Reference: 064582-CUR-00-XX-RP-GE-001, 21 November 2022, a phase of development shall not commence until the following information has been submitted for approval in writing by the City Council, as Local Planning Authority, to identify and evaluate all potential sources and impacts of any ground contamination, groundwater contamination and/or ground gas relevant to the site

- Provision of the missing Historic England plan from Appendix A showing the location of the Manchester Salford Canal arm under the site.
- Submission of Site Investigation Proposals
- Submission of a Site Investigation and Risk Assessment Report
- Submission of a Remediation Strategy

b) When each phase of the development commences, the development shall be carried out in accordance with the previously agreed Remediation Strategy as approved in part (a) of this planning condition.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected and appropriate remedial action is taken in the interests of public safety, pursuant to policies DM1 and EN18 of the Core Strategy.

9) Prior to the first occupation of each phase of the development, and following completion of the remediation strategy approved as part of condition (8), a Completion/Verification Report shall be submitted to and approved in writing by the City Council as Local Planning Authority. This shall demonstrate that the completion of works has been carried out in accordance with the approved remediation strategy and has been effective. The report shall include results of sampling and monitoring carried out in accordance with the approved verification plan to demonstrate that the site remediation criteria have been met.

Reason - To ensure that the site has been appropriately remediated prior to the commencement of works associated with the redevelopment of the site, pursuant to policies EN17, EN18 and DM1 of the Manchester Core Strategy (2012).

10) In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified as part of a phase of development, are found to be present on the site at any time before the development is occupied, then development shall cease and/or the development shall not be occupied until, a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy) is submitted for approval in writing by the City Council, as Local Planning Authority, and the development shall be carried out in accordance with the Revised Remediation Strategy, which shall take precedence over any Remediation Strategy or earlier Revised Remediation Strategy. The approved strategy shall then be implemented and then verified as required condition 9.

Reason - To ensure that the works to be undertaken do not contribute to, or adversely affect, unacceptable levels of water pollution from previously unidentified contamination sources pursuant to policies EN17 and EN18 of the Manchester Core Strategy (2012).

11) A programme of archaeological works shall be undertaken in line with the 'Great Northern, Deansgate, Manchester: Written Scheme of Investigation (WSI) for an Archaeological Watching Brief' (Salford Archaeology, dated 16 July 2018). The works are to be undertaken in accordance with the WSI, which covers the following:

1. A phased programme and methodology of investigation and recording to include:

- an archaeological watching brief to record below-ground remains affected by development ground works.

2. A programme for post investigation assessment to include:

- analysis of the site investigation records and finds

- production of a final report on the significance of the archaeological and historical interest represented.

3. Dissemination of the results commensurate with their significance, including a scheme of heritage display/interpretation.

4. Provision for archive deposition of the report and records of the site investigation.

5. Nomination of a competent person or persons / organisation to undertake the works set out within the approved WSI.

Reason: To record and advance understanding of heritage assets impacted on by the development and to make information about the heritage interest publicly accessible pursuant to policies EN3 of the Manchester Core Strategy (2012) and saved policy DC20 of the Unitary Development Plan for the City of Manchester (1995).

12) Prior to the commencement of a phase of the development, a detailed construction management plan outlining working practices during construction for that phase of the development has been submitted to and approved in writing by the Local Planning Authority, which for the avoidance of doubt should include;

- Display of an emergency contact number;
- Details of Wheel Washing;
- Dust and dirt suppression measures;
- Highway dilapidation survey;
- Compound locations where relevant;
- Consultation with local residents;
- Location, removal/loading, storage and recycling of waste, plant and materials;
- Routing strategy and swept path;
- Parking of construction vehicles and staff;
- Sheeting over of construction vehicles;

Manchester City Council encourages all contractors to be 'considerate contractors' when working in the city by being aware of the needs of neighbours and the environment. Membership of the Considerate Constructors Scheme is highly recommended.

A phase of the development shall be carried out in accordance with the approved construction management plan.

Reason - To safeguard the amenities of nearby residents, highway safety and the safety and operations of the adjacent tramway, pursuant to policies SP1, EN9, EN19 and DM1 of the Manchester Core Strategy (July 2012).

13) Prior to the commencement of a phase of the development, all materials to be used on all external elevations of that phase of the development shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall

include a schedule of all samples and specifications of all materials to be provided (including the inspection of a panel) to be used on all external elevations of the development along with jointing and fixing details, vents, details of the drips to be used to prevent staining in, ventilation and a strategy for quality control management.

The approved materials shall then be implemented as part of the development.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Core Strategy (2012).

14) a) Notwithstanding the Place Strategy including Local Benefits Proposal prepared by Trilog received by the City Council, as Local Planning Authority, prior to the commencement of a phase of the development, details of a Local Benefit Proposal, in order to demonstrate commitment to recruit local labour for the duration of the construction that phase of the development, shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved document shall be implemented as part of the construction of the development.

In this condition a Local Benefit Proposal means a document which includes:

- i) the measures proposed to recruit local people including apprenticeships
- ii) mechanisms for the implementation and delivery of the Local Benefit Proposal
- iii) measures to monitor and review the effectiveness of the Local Benefit Proposal in achieving the objective of recruiting and supporting local labour objectives

(b) Within one month prior to construction work associated with a phase of the development being completed, a detailed report which takes into account the information and outcomes about local labour recruitment pursuant to items (i) and (ii) above shall be submitted for approval in writing by the City Council as Local Planning Authority.

Reason – The applicant has demonstrated a commitment to recruiting local labour pursuant to policies SP1, EC1 and DM1 of the Manchester Core Strategy (2012).

15) Notwithstanding drawings 2891-PLA-XX-00-DR-L-0001, 2891-PLA-XX-00-DR-L-0002, 2891-PLA-XX-00-DR-L-1000, 2891-PLA-XX-00-DR-L-2000, 2891-PLA-XX-00-DR-L-3000, 2891-PLA-XX-00-DR-L-3001, 2891-PLA-XX-00-DR-L-4000, 2891-PLA-XX-01-DR-L-0001, 2891-PLA-XX-01-DR-L-1000, 2891-PLA-XX-01-DR-L-2000, 2891-PLA-XX-01-DR-L-4000, 2891-PLA-XX-02-DR-L-0001, 2891-PLA-XX-02-DR-L-1000, 2891-PLA-XX-02-DR-L-2000, 2891-PLA-XX-02-DR-L-3000, 2891-PLA-XX-02-DR-L-4000, 2891-PLA-XX-12-DR-L-0001, 2891-PLA-XX-12-DR-L-1000, 2891-PLA-XX-12-DR-L-2000 and 2891-PLA-XX-12-DR-L-4000 received by the City Council, as Local Planning Authority, on the 24 November 2022, (a) prior to any above ground works associated with a phase of the development, details of the public and private realm works relating to this phase shall be submitted for approval in writing by the City Council as Local Planning Authority. The details shall include submission and implementation timeframes for the following details:

- (i) Details of the proposed hard landscape materials;
- (ii) Details of the materials, including natural stone or other high quality materials to be used for the reinstatement of the pavements and for the areas between the pavement and the line of the proposed building;
- (iii) Details of the proposed tree species within the public realm including proposed size, species and planting specification including tree pits and design;
- (iv) Details of measures to create potential opportunities to enhance and create new biodiversity within the development to include bat boxes and bricks, bird boxes and appropriate planting and green screens and walls to podium areas;
- (v) Details of the proposed street furniture including seating, bins, boundary treatment, lighting and recreational areas including children's play;
- (vi) Details of any external steps and handrails;
- (vii) A strategy providing details of replacement tree planting, including details of overall numbers, size, species and planting specification, constraints to further planting and details of on-going maintenance.

(b). The approved details shall then be implemented and be in place prior to the first occupation of the phase of the development hereby approved.

If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted at the same place.

The boundary treatment shall be retained and maintained in situ thereafter and notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking or re-enacting that Order with or without modification) no boundary treatment shall be erected on site, other than that shown on the approved plans.

Reason - To ensure that a satisfactory landscaping scheme for the development is carried out that respects the character and visual amenities of the area, in accordance with policies R1.1, I3.1, T3.1, S1.1, E2.5, E3.7 and RC4 of the Unitary Development Plan for the City of Manchester and policies SP1, DM1, EN1, EN9 EN14 and EN15 of the emerging Core Strategy.

16) Prior to the first occupation/use of a phase of the development, a detailed landscaped management plan for that phase shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of how the public realm and hard and soft landscaping areas for the relevant phase will be maintained including maintenance schedules and repairs. The management plan shall then be implemented as part of the development and remain in place for as long as the development remains in use.

Reason - To ensure that the satisfactory landscaping scheme for the development is maintained in the interest of the character and visual amenities of the area, in accordance with policies SP1, EN9 and DM1 of the Core Strategy

17) Prior to the first occupation/use of a phase of the development, details of the implementation, maintenance and management of the sustainable drainage scheme for that phase shall be submitted for approval in writing by the City Council, as Local Planning Authority.

For the avoidance of doubt the scheme shall include the following:

- Verification report providing photographic evidence of construction; and
- Management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime.

The approved scheme shall then be implemented in accordance with the details and thereafter managed and maintained for as long as the development remains in use.

Reason - To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution pursuant to policies SP1, EN14 and DM1 of the Manchester Core Strategy (2012).

18) Each phase of development shall be carried out in accordance with the Environmental Standards Statement including BREEAM Pre-Assessment prepared by WSP and Energy Statement prepared by WSP received by the City Council, as Local Planning Authority, on the 24 November 2022.

A post construction review certificate/statement for the phase shall be submitted for approval in writing, within a timescale that has been previously agreed in writing, to the City Council as Local Planning Authority for that phase.

Reason - In order to minimise the environmental impact of the development pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Core Strategy and the principles contained within The Guide to Development in Manchester SPD (2007) and the National Planning Policy Framework.

19) Phase A of the development hereby approved shall achieve a post-construction Building Research Establishment Environmental Assessment Method (BREEAM) rating of at least a 'Excellent' rating. Post construction review certificate(s) shall be submitted to, and approved in writing by the City Council as local planning authority, within three months of the buildings hereby approved being first occupied.

Reason - In order to minimise the environmental impact of the development, pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Core Strategy, and the principles contained within The Guide to Development in Manchester SPD (2007), and the National Planning Policy Framework.

20) Notwithstanding Chapter 9: Noise and Vibration of the ES stamped as received by the City Council, as Local Planning Authority, on the 24 November 2024, (a) Prior to the first occupation/use of each phase of the development hereby approved, details of any externally mounted ancillary plant, equipment and servicing shall be submitted for approval in writing by the City Council, as Local Planning Authority.

Externally mounted plant, equipment and servicing shall be selected and/or acoustically treated in accordance with a scheme designed so as to achieve a rating level of 5 dB (L_{aeq}) below the typical background (LA₉₀) level at the nearest noise sensitive location.

(b) Prior to the first occupation/use of each phase of the development, a verification report will be required to validate that the work undertaken conforms to the recommendations and requirements approved as part of part (a) of this planning condition. The verification report shall include post completion testing to confirm the noise criteria has been met. In instances of non-conformity, these shall be detailed along with mitigation measures required to ensure compliance with the noise criteria. A verification report and measures shall be agreed until such a time as the development complies with part (a) of this planning condition.

Any mitigation measures shall be implemented in accordance with a timescale to be agreed with the City Council, as Local Planning Authority. Any measures shall thereafter retained and maintained in situ.

Reason - To minimise the impact of plant on the occupants of the development pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012) and saved policy DC26 of the Unitary Development Plan for the City of Manchester (1995).

21) (a) Notwithstanding Chapter 9: Noise and Vibration of the ES stamped as received by the City Council, as Local Planning Authority, on the 24 November 2022, prior to the first use of commercial units, residents lounges, offices and co working spaces and gymnasium (and any other relevant uses) in each phase of the development, a scheme of acoustic insulation for those spaces shall be submitted for approval in writing by the City Council, as Local Planning Authority.

Where entertainment noise is proposed the LA_{eq} (entertainment noise) shall be controlled to 5dB below the LA₉₀ (without entertainment noise) in each octave band at the facade of the nearest noise sensitive location, and internal noise levels at structurally adjoined residential properties in the 63Hz and 125Hz octave frequency bands shall be controlled so as not to exceed (in habitable rooms) 47dB and 41dB (L_{eq},5min), respectively

(b) Prior to the first use of those spaces within a relevant phase of the development, a verification report will be required to validate that the work undertaken conforms to the recommendations and requirements approved as part of part (a) of this planning condition. The verification report shall include post completion testing to confirm the noise criteria has been met. In instances of non-conformity, these shall be detailed along with mitigation measures required to ensure compliance with the noise criteria. A verification report and measures shall be agreed until such a time as the development complies with part (a) of this planning condition.

Any mitigation measures shall be implemented in accordance with a timescale to be agreed with the City Council, as Local Planning Authority. Any measures shall thereafter retained and maintained in situ.

Reason - In order to limit the outbreak of noise from the commercial premises pursuant to policies SP1 and DM1 of the Core Strategy (2007) and saved policy DC26 of the Unitary Development Plan for the City of Manchester (1995).

22) (a) Notwithstanding Chapter 9: Noise and Vibration of the ES stamped as received by the City Council, as Local Planning Authority, on the 24 November 2022, prior to the any above ground works, excluding demolition, associated with phases B/C, a scheme for acoustically insulating the proposed residential accommodation against noise from Deansgate and Peter Street shall be submitted for approval in writing by the City Council as Local Planning Authority.

There may be other actual or potential sources of noise which require consideration on or near the site, including any local commercial/industrial premises such as Manchester Central and bars/music venues within the Great Northern complex and surrounding roads.

The potential for overheating shall also be assessed and the noise insulation scheme shall take this into account.

Noise survey data shall include measurements taken during a rush-hour period and night time to determine the appropriate sound insulation measures necessary. The following noise criteria shall be required to be achieved when providing adequate ventilation as defined by Approved Document F of the Building Regulations (whole dwelling ventilation):

Bedrooms (night time - 23.00 - 07.00) 30 dB L_{Aeq} (individual noise events shall not exceed 45 dB $L_{Amax,F}$ by more than 15 times)

Living Rooms (daytime - 07.00 - 23.00) 35 dB L_{Aeq}

Gardens and terraces (daytime) 55 dB L_{Aeq}

Higher internal noise levels than those specified above may be allowed when higher rates of ventilation are required in relation to the overheating condition.

Additionally, where entertainment noise is a factor in the noise climate the sound insulation scheme shall be designed to achieve internal noise levels in the 63Hz and 125Hz octave centre frequency bands so as not to exceed (in habitable rooms) 47dB and 41dB ($L_{eq,5min}$), respectively.

The approved noise insulation and ventilation scheme shall be completed before the first occupation of the residential accommodation within phase B/C of this development.

(b) Prior to the first occupation of the residential accommodation within phase B/C, a verification report will be required to validate that the work undertaken conforms to the recommendations and requirements approved as part of part (a) of this planning condition. The verification report shall include post completion testing to confirm the noise criteria has been met with windows and purge vent doors closed. In instances of non-conformity, these shall be detailed along with mitigation measures required to

ensure compliance with the noise criteria. A verification report and measures shall be agreed until such a time as the development complies with part (a) of this planning condition.

Any mitigation measures shall be implemented in accordance with a timescale to be agreed with the City Council, as Local Planning Authority. Any measures shall thereafter retained and maintained in situ.

Reason: To secure a reduction in noise from traffic or other sources in order to protect future residents from noise disturbance pursuant to policies SP1, H1 and DM1 of the Core Strategy (2007) and saved policy DC26 of the Unitary Development Plan for the City of Manchester (1995).

23) Notwithstanding the Waste Management Strategy prepared by Curtins received by the City Council, as Local Planning Authority on the 24 November 2022, prior to any above ground works, excluding demolition, details a waste management strategy for the storage and disposal of refuse for the residential element of phase B/C shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved scheme shall be implemented prior to the first occupation of the residential element of phase B/C and shall remain in situ whilst the use or development is in operation.

Reason - To ensure adequate refuse arrangement are put in place for the residential element of the scheme pursuant to policies EN19 and DM1 of the Manchester Core Strategy.

24) The commercial uses as part of the phases of the development shall be carried out in accordance with the Waste Management Strategy prepared by Curtins received by the City Council, as Local Planning Authority on the 24 November 2022. The approved details shall then be implemented for as long as the development is in use.

Reason - To ensure adequate refuse arrangement are put in place for the non residential elements of the scheme pursuant to policies EN19 and DM1 of the Manchester Core Strategy.

25) Prior to the first use of new commercial units within each phase of the development, details of a scheme to extract fumes, vapours and odours from these spaces shall be submitted for approval in writing by the City Council, as Local Planning Authority (unless no kitchen extraction or cooking facilities are required). The approved scheme shall then be implemented prior to the first occupation of each of these spaces and thereafter retained and maintained in situ.

Reason - To ensure appropriate fume extraction is provided for the non residential spaces pursuant to policies SP1 and DM1 of the Manchester Core Strategy and saved policy DC10 of the Unitary Development Plan for the City of Manchester (1995).

26) The development hereby approved shall include a building and site lighting scheme and a scheme for the illumination of external areas during the period

between dusk and dawn. Prior to the first occupation of a phase of the development, full details of such a scheme for that phase shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved scheme shall be implemented in full prior to the first occupation/use of a phase of the development and shall remain in operation for so long as the development is occupied.

Reason - In the interests of amenity, crime reduction and the personal safety of those using and ensure that lighting is installed which is sensitive to the bat environment the proposed development in order to comply with the requirements of policies SP1 and DM1 of the Core Strategy.

27) If any lighting at the development hereby approved, when illuminated, causes glare or light spillage which in the opinion of the Council as local planning authority causes detriment to adjoining and nearby residential properties, within 21 days of a written request, a scheme for the elimination of such glare or light spillage shall be submitted to the Council as local planning authority and once approved shall thereafter be retained in accordance with details which have received prior written approval of the City Council as Local Planning Authority.

Reason - In order to minimise the impact of the illumination of the lights on the occupiers of nearby residential accommodation, pursuant to policies SP1 and DM1 of the Core Strategy.

28) Deliveries, servicing and collections including waste collections shall not take place outside the following hours for a phase of the development:

Monday to Saturday 07:30 to 20:00
Sundays (and Bank Holidays): 10:00 to 18:00

Reason - In the interest of residential amenity pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

29) Prior to the first use of the commercial units within a phase of the development, a schedule of opening hours shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved hours shall then be implemented and thereafter the uses shall operate in accordance with them.

There shall be no amplified sound or any amplified music at any time within these spaces unless it can be shown as part of condition 21 that there would be no unacceptable impact on residential amenity.

Reason - In interests of residential amenity in order to reduce noise and general disturbance in accordance with saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

30) Prior to the first use of the commercial spaces within a phase of the development, details of any external areas associated with these commercial spaces (including an Operating Schedule) shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The Operating Schedule shall contain the following details:

- a. A scaled layout plan showing the proposed seating area, including layout of furniture and demarcation of the area;
- b. Full details of the measures proposed to ensure that the proposed seating area is fully accessible by disabled people;
- c. Details of the proposed furniture, including any barriers;
- d. A detailed management strategy that includes information on how the proposed external seating area would be managed in terms of potential noise disturbance, additional movement and activity, litter and storage of furniture at night;
- e. days and hours of operation.

The approved plan shall be implemented upon first use of the commercial uses in that phase and thereafter retained.

No amplified sound or any music shall be produced or played in any part of the site outside the building.

Reason - To safeguard the amenities of the occupiers of nearby properties, pursuant to policies SP1 and DM1 of the Core Strategy.

31) Each commercial unit, within each phase shall remain as one unit and shall not be sub divided or amalgamated without the benefit of planning permission being secured.

Reason- In the interests of residential amenity and to ensure the future viability and vitality of the commercial units pursuant to saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies DM1, C5 and SP1 of the Manchester Core Strategy.

32) The commercial units within each phase of development shall be occupied as Class E (excluding convenience retail) and Sui Generis: Drinking Establishment/takeaway) and for no other purpose of The Town and Country Planning (Use Classes) Order 1987 (or any order revoking and re-enacting that Order with or without modification).

The commercial units within Deansgate Terrace (southern section) shall be occupied as Class E (commercial, business and service) (excluding convenience retail), F1 (Learning and non residential institutions), F2 (Local Community) or Sui Generis (drinking establishments / hot food takeaway / live music venue) and for no other purpose of The Town and Country Planning (Use Classes) Order 1987 (or any order revoking and re-enacting that Order with or without modification).

Reason - For the avoidance of doubt and in order to secure a satisfactory form of development due to the particular circumstance of the application site, ensuring the

vitality of the units and in the interest of residential amenity, pursuant policy DM1 of the Core Strategy for Manchester (2012).

33) In the event that any of the commercial unit in a phase of the development is occupied as a café/restaurant, drinking establishment live music, prior to their first use the following details must be submitted and agreed in writing by the City Council, as Local Planning Authority. These details are as follows:

- Management of patrons and control of external areas. For the avoidance of doubt this shall include:
 - o Dispersal policy;
 - o Mechanism for ensuring windows and doors remain closed save for access or egress after 9pm

The approved scheme shall be implemented upon first use of the premises and thereafter retained and maintained.

Reason - To safeguard the amenities of nearby residential occupiers as the site is located in a residential area, pursuant to policies SP1, DM1 and C10 of the Manchester Core Strategy (2012) and to saved policy DC26 of the Unitary Development Plan for Manchester.

34) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (or any order revoking and re-enacting that Order with or without modification) no part of the residential floorspace (Class C3(a)) in phase B/C shall be used for any purpose other than the purpose(s) of Class C3(a) of the Schedule to the Town and Country Planning (Use Classes) Order 1987 (as amended) (or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification). this does not preclude two unrelated people sharing a property.

Reason - In the interests of residential amenity, to safeguard the character of the area and to maintain the sustainability of the local community through provision of accommodation that is suitable for people living as families pursuant to policies DM1 and H11 of the Core Strategy for Manchester and the guidance contained within the National Planning Policy Framework.

35) The residential use hereby approved in phase B/C shall be used only as private dwellings (which description shall not include serviced properties or similar uses where sleeping accommodation (with or without other services) is provided by way of trade for money or money's worth and occupied by the same person for less than ninety consecutive nights) and for no other purpose (including any other purpose in Class C3 of the Schedule to the Town and Country Planning (Use Classes) Order 1987 (as amended), or any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification).

Reason - To safeguard the amenities of the neighbourhood by ensuring that other uses which could cause a loss of amenity such as serviced apartments/apart hotels do not commence without prior approval; to safeguard the character of the area, and to maintain the sustainability of the local community through provision of

accommodation that is suitable for people living as families pursuant to policies DM1 and H11 of the Core Strategy for Manchester and the guidance contained within the National Planning Policy Framework.

36) The development shall be carried out in accordance with the Crime Impact Statement prepared by Design for Security at Greater Manchester Police received by the City Council, as Local Planning Authority, on the 24 November 2022. The development shall only be carried out in accordance with these approved details. Prior to the first occupation of each phase of the development the City Council, as Local Planning Authority, must acknowledge in writing that it has received written confirmation of a Secured by Design accreditation.

Reason - To reduce the risk of crime pursuant to policies SP1 and DM1 of the Core Strategy and to reflect the guidance contained in the National Planning Policy Framework.

37) A phase of the development shall be carried out in accordance with the Travel Plan Framework prepared by Curtins received by the City Council, as Local Planning Authority, on the 24 November 2022.

In this condition a Travel Plan means a document which includes:

- i) the measures proposed to be taken to reduce dependency on the private car by those living at the development;
- ii) a commitment to surveying the travel patterns of residents/staff during the first three months of the first use of the building and thereafter from time to time
- iii) mechanisms for the implementation of the measures to reduce dependency on the private car
- iv) measures for the delivery of specified Travel Plan services
- v) measures to monitor and review the effectiveness of the Travel Plan in achieving the objective of reducing dependency on the private car

Within six months of the first occupation/use of a phase of the development, a Travel Plan for that phase which takes into account the information about travel patterns gathered pursuant to item (ii) above shall be submitted for approval in writing by the City Council as Local Planning Authority. Any Travel Plan which has been approved by the City Council as Local Planning Authority shall be implemented in full at all times when the development hereby approved is in use.

Reason - To assist promoting the use of sustainable forms of travel for residents, pursuant to policies T1, T2 and DM1 of the Manchester Core Strategy (2012).

38) Prior to the first occupation/use of a phase of the development, the cycle storage, as indicated on drawings 10422-SHP-Z4-A-B5D9-G200-PL-00-001 and 10422-SHP-Z4-A-B5D9-G200-PL-01-001 received by the City Council, as Local Planning Authority, on the 27 November 2022 shall be implemented and made available upon first occupation/use of the residential and office elements of the development and thereafter retained and maintained in situ.

Reason - To ensure there is sufficient cycles stand provision at the development and the residents in order to support modal shift measures pursuant to policies SP1, T1, T2 and DM1 of the Manchester Core Strategy (2012).

39) Prior to the first occupation of the residential element within phase B/C the car parking layout as indicated on drawings 10422-SHP-Z4-A-B5D9-G200-PL-00-001, 10422-SHP-Z4-A-B5D9-G200-PL-01-001, 10422-SHP-Z4-A-B5D9-G200-PL-02-001 and 10422-SHP-Z4-A-B5D9-G200-PL-03-001 stamped as received by the City Council, as Local Planning Authority, on the 27 November 2022 shall be implemented and made available. The car parking shall remain available for as long as the residential element remains in use.

Reason - To ensure sufficient car parking is available for the occupants of the office element of the development pursuant to policies SP1, T1, and DM1 of the Manchester Core Strategy (2012).

40) Prior to the first occupation of the residential element within phase B/C, an electric vehicle car parking strategy for the provision electric car charging to the car parking spaces as indicated on drawings stamped as received by the City Council, as Local Planning Authority, on the 24 November 2022 shall be submitted for approval in writing by the City Council, as Local Planning Authority. The scheme shall include the following details:

- (a) Layout and location of electric vehicle charging points;
- (b) Provision for 10% of the spaces to be fitted with a 7kw fast charging point;
- (c) Provision 10% car parking spaces would be fitted with appropriate infrastructure to be adapted at a future date should demand be shown as part of the travel plan review required by planning condition 37.

The electric vehicle car parking strategy shall be implemented prior to the first occupation of phase B/C and retained and maintained in situ for as long as the development remains in use.

Reason – In the interest of minimise the impact on local air quality conditions pursuant to policy EN16 of the Manchester Core Strategy (2012).

41) Prior to the first occupation/use of a phase of the development, a scheme of highway works and footpaths reinstatement/public realm for that phase shall be submitted for approval in writing by the City Council, as Local Planning Authority.

This shall include the following:

- Works to the junction at Great Bridgewater Street and Watson Street including widening footway widths, replacement of the staggered controlled crossing with a continuous crossing with an additional crossing at Great Bridgewater Street together with preventing through movement along Great Bridgewater Street;
- A scheme to prevent through movement along Great Bridgewater Street including Moving Traffic Enforcement cameras to enforce the no through route works;

- Reduction in carriageway width along Watson Street;
- Reconfiguration of the loading bays along Watson Street;
- Provision of tactile paving and dropped kerbs and reinstatement of redundant access points;
- Enhanced areas of public realm, tree and shrub planting along Watson Street;
- - Installation of wind mitigation measures including screens/planters.

The approved scheme for that phase shall be implemented and be in place prior to the first occupation/use of that phase of the development.

Reason - To ensure safe access to the development site in the interest of pedestrian and highway safety pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012).

42) Notwithstanding the Television Baseline Survey Report (pre-development) prepared by Pager Power, received by the City Council, as Local Planning Authority, on the 24 November 2022, within one month of the practical completion of each phase of the development, and at any other time during the construction of the development if requested in writing by the City Council as Local Planning Authority, in response to identified television signal reception problems within the potential impact area a study to identify such measures necessary to maintain at least the pre-existing level and quality of signal reception identified in the survey carried out above for that phase shall be submitted for approval in writing by the City Council, as Local Planning Authority. The measures identified must be carried out either before a phase of the development is first occupied (or brought into use) or within one month of the study being submitted for approval in writing to the City Council as Local Planning Authority, whichever is the earlier.

Reason - To provide an indication of the area of television signal reception likely to be affected by the development to provide a basis on which to assess the extent to which the development during construction and once built, will affect television reception and to ensure that the development at least maintains the existing level and quality of television signal reception - In the interest of residential amenity, as specified in policy DM1 of Core Strategy.

43) Prior to the first occupation/use of a phase of development, details of bird and bat boxes to be provided (including location and specification) in that phase shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved details shall then be implemented prior to the first occupation/use of phase and thereafter retained and maintained in situ.

Reason – To provide new habitats for birds and bats pursuant to policies SP1 and EN15 of the Manchester Core Strategy (2012).

44) Notwithstanding the General Permitted Development Order 2015 as amended by the Town and Country Planning (Permitted Development and Miscellaneous Amendments) (England) (Coronavirus) Regulations 2020 or any legislation amending or replacing the same, no further development in the form of upward extensions to the building in phase B shall be undertaken other than that expressly authorised by the granting of planning permission.

Reason - In the interests of protecting residential amenity and visual amenity of the area in which the development is located pursuant to policies DM1 and SP1 of the Manchester Core Strategy.

45) Prior to the first occupation/use of a phase of the development a signage strategy for external facades and commercial frontages shall be submitted for approval in writing by the City Council, as Local Planning Authority. The signage strategy will include timescales for implementation. The approved strategy shall then be implemented for that phase and used to inform any future advertisement applications for the building.

Reason – In the interest of visual amenity pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

46) All windows at ground level, unless shown otherwise on the approved drawings detailed in condition 2, shall be retained as a clear glazed window opening at all times and views into the premises shall not be screened or obscured in any way.

Reason - The clear glazed window(s) is an integral and important element in design of the ground level elevations and are important in maintaining a visually interesting street-scene consistent with the use of such areas by members of the public, and so as to be consistent with saved policy DC14 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

47) The development hereby approved shall include for full disabled access to be provided to the public realm and communal walkways and via the main entrances and to the floors above.

Reason - To ensure that satisfactory disabled access is provided by reference to the provisions Core Strategy policy DM1.

48) Prior to the first occupation/use of a phase of development, details of the siting, scale and appearance of the wind ,mitigation measures as required by Chapter 10: Wind of the ES stamped as received by the City Council, as Local Planning Authority, on the 24 November 2022 shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved details shall be implemented prior to the first occupation/use of a phase of the development and thereafter retained and maintained in situ for as long as the development is in use.

Reason – In the interest of pedestrians safety and to ensure that the wind conditions are acceptable pursuant to policy DM1 of the Manchester Core Strategy (2012).

49) Prior to the first use of phase A and B/C (Great Northern Warehouse and Deansgate Terrace only) the development hereby approved, details of the siting, scale and appearance of the solar panels to the roof of the buildings (including cross sections) shall be submitted to the City Council, as Local Planning Authority. The approved details shall then be implemented prior to the first use of the relevant phase of the development and thereafter retained and maintained in situ.

Reason - In the interest of ensuring the solar panels are installed and to ensure that they are appropriate in terms of visual amenity pursuant to policies SP1, EN1, EN6 and DM1 of the Manchester Core Strategy (2012).

50) Prior to the first use of phase A and B/C (Great Northern and Deansgate Terrace only), details of the siting, scale and appearance of the air source heat pumps to the buildings hereby approved. The air source heat pumps must also comply with the noise criteria as specified in condition 20. The approved details shall then be implemented prior to the first use of the development and thereafter retained and maintained in situ.

Reason - In the interest of ensuring the air source heat pumps are installed and to ensure that they are appropriate in terms of visual amenity pursuant to policies SP1, EN1, EN6 and DM1 of the Manchester Core Strategy (2012).

51) Prior to the first use of each of the commercial units in each phase, details of any roller shutters to the ground floor of the premises shall be submitted for approval in writing by the City Council, as Local Planning Authority. The shutters shall be fitted internally to the premises. The approved details shall be implemented prior to the first occupation of each of the commercial units and thereafter retained and maintained in situ.

Reason - To ensure that the roller shutters are appropriate in visual amenity terms pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012).

52) Prior to the first use of the roof terraces and podium for each phase, details of the opening hours for the terraces and podium shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved opening hours shall then become the operating hours for the terraces in that phase

There shall be no amplified music or sound on the roof terrace at any time.

Reason - In interests of amenity in order to reduce noise and general disturbance in accordance with saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

53) Prior to the commencement of a phase of the development, a dilapidation survey of surrounding roads is submitted for approval in writing by the City Council, as Local Planning Authority. The survey should include photographs and commentary on the condition of carriageway / footways on construction vehicle routes surrounding the site. Within two months of the completion of construction works, a post development dilapidation survey shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include remedial measures to re-surface and repair carriageways where necessary. Any remedial works shall be undertaken within two months of approval.

Reason - In the interest of minimising the impact on the condition of the carriageway pursuant to policy DM1 of the Manchester Core Strategy (2012).

54) Prior to the first occupation/use of a phase of the development, a servicing strategy shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall detail how coordination of vehicle arrival/departures to ensure operations are scheduled to avoid vehicles stacking on the adopted highway and ensuring that servicing takes place outside of the network AM and PM peak periods.

The approved strategy shall be implemented upon the first occupation/use of the development and remain in place.

Reason – In the interest of finding an appropriate servicing strategy in the interest of highway and pedestrians safety pursuant to policy DM1 of the Manchester Core Strategy (2012).

55) Prior to the first occupation of the residential element within phase B/C, details of car club spaces within the car parking layout as indicated on drawings 10422-SHP-Z4-A-B5D9-G200-PL-00-001, 10422-SHP-Z4-A-B5D9-G200-PL-01-001, 10422-SHP-Z4-A-B5D9-G200-PL-02-001 and 10422-SHP-Z4-A-B5D9-G200-PL-03-001 stamped as received by the City Council, as Local Planning Authority, on the 27 November 2022 shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved details shall be implemented and made available prior to the first occupation of the development and thereafter retained and maintained in situ.

Reason - To ensure car club parking is available for the occupants of the development pursuant to policies SP1, T1, and DM1 of the Manchester Core Strategy (2012).

56) Prior to the first occupation of the residential element within phase B/C, details of disabled parking within the car parking layout as indicated on drawings 10422-SHP-Z4-A-B5D9-G200-PL-00-001, 10422-SHP-Z4-A-B5D9-G200-PL-01-001, 10422-SHP-Z4-A-B5D9-G200-PL-02-001 and 10422-SHP-Z4-A-B5D9-G200-PL-03-001 stamped as received by the City Council, as Local Planning Authority, on the 27 November 2022 shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include a car management strategy for the car parking and disabled bays to understand the management arrangements for the residential and general public parking together with ensuring that disabled bays are available for residents to occupy. The approved details shall be implemented and made available prior to the first occupation of the development and thereafter retained and maintained in situ.

Reason - To ensure disabled parking is available for the occupants of the development pursuant to policies SP1, T1, and DM1 of the Manchester Core Strategy (2012).

57) Prior to the commencement of the development, an existing car park survey work shall be undertaken and submitted for approval in writing by the City Council. This shall detail where car parking may be displaced to along the highway network and the data used to inform highway improvements works outlined in condition 41.

Reason -- To ensure appropriate highway improvements are undertaken around the development in the interest of pedestrian and highway safety pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012).

Informatives

- Any signage, wayfinding, banners or any other advertisements to be installed in and around the application site for the purpose of the promotion of the developments and routes to it may require consent under the Town and Country Planning (Control of Advertisements) (England) Regulations 2007.
- The applicant's attention is drawn to the new procedures for crane and tall equipment notifications, please see:
<https://www.caa.co.uk/Commercial-industry/Airspace/Event-and-obstacle-notification/Crane-notification/>
- It is important that any conditions or advice in this response are applied to a planning approval. Where a Planning Authority proposes to grant permission against the advice of Manchester Airport, or not attach conditions which Manchester Airport has advised, it shall notify Manchester Airport, and the Civil Aviation Authority as specified in the Town & Country Planning (Safeguarded Aerodromes, Technical Sites and Military Explosive Storage Areas) Direction 2002.
- It is expected that all modifications / improvements to the public highway are achieved with a maximum carbon footprint of 40%. Materials used during this process must also be a minimum of 40% recycled and fully recyclable. Developers will be expected to demonstrate that these standards can be met prior to planning conditions being discharged. The developer is to agree the above with MCC's Statutory Approvals and Network Resilience Teams post planning approval and prior to construction taking place
- Regarding S278 agreements a deposit is required to begin an application, additional costs will be payable and are to be agreed with S278 team. A S278 is required for works to the adopted highway, minimum standard S278 technical approval timescale is between 4-6 months, TRO's can take 10-12 months. An independent 'Stage 2' Road Safety Audit will be required and the design may require changes if any issues are raised with all costs attributable to the Developer. A 'Stage 1' Road Safety Audit should be completed during the planning stage and a copy of the report (with Designer's Response) is to be made available to the Statutory Approvals Team upon request.
- You should ensure that the proposal is discussed in full with Building Control to ensure they meet with the guidance contained in the Building Regulations for fire safety. Should it be necessary to change the development due to conflicts with Building Regulations, you should also discuss the changes with the Planning team to ensure they do not materially affect your permission.
- Whilst the building to be demolished has been assessed as negligible risk for bats, the applicant is reminded that under the 2019 Regulations it is an

offence to disturb, harm or kill bats. If a bat is found during demolition all work should cease immediately and a suitably licensed bat worker employed to assess how best to safeguard the bat(s). Natural England should also be informed

- The applicant is reminded that, under the Wildlife and Countryside Act 1981 as amended it is an offence to remove, damage, or destroy the nest of a wild bird, while the nest is in use or being built. Planning consent does not provide a defence against prosecution under this act. If a birds nest is suspected work should cease immediately and a suitably experienced ecologist employed to assess how best to safeguard the nest(s).

Listed Building Consent 135583/LO/2022 'Deansgate Terrace'

Recommendation Minded to Approve subject to the completion of the legal agreement associated with planning permission 135565/FO/2022

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. Pre application advice has been sought in respect of this matter where early discussions took place regarding the works to the listed building. The proposal is considered to be acceptable and therefore determined within a timely manner.

Conditions of approval for Listed Building Consent 135583/LO/2022

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 18 of the Planning (Listed Buildings and Conservation Areas) Act 1990.

2) The demolition and alteration to Deansgate Terrace shall not be undertaken before a contract for the carrying out of the building works have been made, and planning permission has been granted for the redevelopment for which the contract provides, and evidence of that contract has been supplied to the City Council as Local Planning Authority.

Reason - In the interests of visual amenity and for the avoidance of doubt, and to ensure that redevelopment of the site takes place following external alterations to the existing building pursuant to policy EN3 of the Manchester Core Strategy and saved DC19 of the Unitary Development Plan for the City of Manchester, and the National Planning Policy Framework.

3) The development hereby approved shall be carried out in accordance with the following drawings and documents:

Drawings

10422-SHP-Z3A-A-B5D9-JC20-XP-B1-001, 10422-SHP-Z3A-A-B5D9-JC20-XP-00-001, 10422-SHP-Z3A-A-B5D9-JC20-XP-01-001, 10422-SHP-Z3A-A-B5D9-JC20-XP-02-001, 10422-SHP-Z3A-A-B5D9-JC20-XP-03-001, 10422-SHP-Z3A-A-B5D9-JC20-XP-RF-001, 10422-SHP-Z3A-A-B5D9-JC20-XE-XX-001, 10422-SHP-Z3A-A-B5D9-JC20-XE-EE-001, 10422-SHP-Z3B-A-B5D9-JC20-XP-B1-001, 10422-SHP-Z3B-A-B5D9-JC20-XP-00-001, 10422-SHP-Z3B-A-B5D9-JC20-XP-01-001, 10422-SHP-Z3B-A-B5D9-JC20-XP-02-001, 10422-SHP-Z3B-A-B5D9-JC20-XP-03-001, 10422-SHP-Z3B-A-B5D9-JC20-XP-RF-001, 10422-SHP-Z3B-A-B5D9-JC20-XE-XX-001, 10422-SHP-Z3B-A-B5D9-JC20-XE-EE-001, 10422-SHP-Z3A-A-B5D9-JC20-SC-XX-001, 10422-SHP-Z3A-A-B5D9-G200-PL-B1-001, 10422-SHP-Z3A-A-B5D9-G200-PL-00-001, 10422-SHP-Z3A-A-B5D9-G200-PL-01-001, 10422-SHP-Z3A-A-B5D9-G200-PL-02-001, 10422-SHP-Z3A-A-B5D9-G200-PL-03-001, 10422-SHP-Z3A-A-B5D9-G200-PL-RF-001, 10422-SHP-Z3A-A-B5D9-G332-PL-B1-001, 10422-SHP-Z3A-A-B5D9-G332-PL-00-001, 10422-SHP-Z3A-A-B5D9-G332-PL-01-001, 10422-SHP-Z3A-A-B5D9-G332-PL-02-001, 10422-SHP-Z3A-A-B5D9-G200-SE-XX-001, 10422-SHP-Z3A-A-B5D9-G200-SE-XX-002, 10422-SHP-Z3A-A-B5D9-G200-EL-XX-001, 10422-SHP-Z3A-A-B5D9-G200-EL-EE-001, 10422-SHP-Z3B-A-B5D9-G200-PL-B1-001, 10422-SHP-Z3B-A-B5D9-G200-PL-00-001, 10422-SHP-Z3B-A-B5D9-G200-PL-01-001, 10422-SHP-Z3B-A-B5D9-G200-PL-02-001, 10422-SHP-Z3B-A-B5D9-G200-PL-03-001, 10422-SHP-Z3B-A-B5D9-G200-PL-RF-001, 10422-SHP-Z3B-A-B5D9-G332-PL-B1-001, 10422-SHP-Z3B-A-B5D9-G332-PL-00-001, 10422-SHP-Z3B-A-B5D9-G332-PL-01-001, 10422-SHP-Z3B-A-B5D9-G332-PL-02-001, 10422-SHP-Z3B-A-B5D9-G200-SE-XX-001, 10422-SHP-Z3B-A-B5D9-G200-EL-XX-001, 10422-SHP-Z3B-A-B5D9-G200-EL-EE-001, 10422-SHP-Z3A-A-B5D9-G240-DE-RF-001, 10422-SHP-Z3A-A-B5D9-G251-DE-EE-001, 10422-SHP-Z3A-A-B5D9-G251-DE-EE-002, 10422-SHP-Z3A-A-B5D9-G251-DE-EE-003, 10422-SHP-Z3A-A-B5D9-G251-DE-EW-001, 10422-SHP-Z3A-A-B5D9-G251-DE-EW-002, 10422-SHP-Z3A-A-B5D9-G251-DE-EW-003, 10422-SHP-Z3A-A-B5D9-G251-DE-XX-001, 10422-SHP-Z3A-A-B5D9-G321-DE-XX-001, 10422-SHP-Z3A-A-B5D9-G321-DE-XX-002 and 10422-SHP-Z3A-A-B5D9-Z330-SC-XX-001

The above documents were received by the City Council, as Local Planning Authority, on the 24 November 2022

Supporting Information

Design & Access Statement prepared by SimpsonHaugh, Public Realm Strategy prepared by Planit.IE, Façade Lighting Strategy prepared by Light Bureau, Stage 2 Civil and Structural Report prepared by Curtins, Condition Surveys prepared by Aura and Quadriga, Archaeological Desk Based Assessment and covering note prepared by Salford Archaeology and Heritage Report prepared by Stephen Levrant.

The above documents were received by the City Council, as Local Planning Authority, on the 24 November 2022

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy.

4) Prior to the commencement of works to remove the following, as indicated within the heritage statement prepared by Stephen Levrant stamped as received by the City Council, as Local Planning Authority, on the 24 November 2022, a detailed method

statement shall be submitted detailing how the fabric will be removed together how the remaining historic fabric will be protected and repaired once removal takes place.

This condition applies to the following works:

- (a) Removal of modern extensions;
- (b) Lighting/CCTV;
- (c) Signage;
- (d) Shopfronts.

The removal works shall be carried out in accordance with the approved details. Should any other parts of the Listed structure become damaged as a consequence of the removal/alteration work then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the method used to remove historic fabric is appropriate and that any damaged to the historic fabric is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

5) Prior to the commencement of repair works to the exterior masonry and stonework including, a detailed method statement and specification (including material specification) for the removal, alteration, repair, re-pointing, cleaning and replacement works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Best endeavours shall be made to use salvage brick work and stonework and other materials wherever possible in the works associated with the development. Should it not be possible to use salvage material in the restoration then notification shall be made to the City Council, as Local Planning Authority, prior to the starting of the works. Suitable materials shall then be agreed with the City Council, as Local Authority, and used where necessary.

Reason - To ensure that a satisfactory restoration and repair of the exterior of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

6) Prior to the commencement of works to repair the rainwater goods to the listed building, a detailed method statement and specification shall be submitted for approval in writing by the City Council, as Local Planning Authority. The repair to the rainwater goods shall be carried out in accordance with this method statement and be completed prior to the first occupation of the development.

Reason - To ensure the rainwater goods are of an acceptable appearance and there is no resulting harm to the Listed building pursuant to policy EN3 of the Manchester Core Strategy (2012).

7) Prior to the repair of windows within the existing listed façade, a detailed method statement and specification for the window repairs shall be submitted for approval in writing by the City Council, as Local Planning Authority. This should also include any removal, alteration and repair of the historic fabric.

The works shall then be carried out in accordance with this method statement and specification as part of the development and be completed prior to the first occupation of the commercial units.

Reason - To ensure that a satisfactory interventions into the Listed Building and retention/repair of as much of the Listed fabric as possible along with appropriate use of materials in the restoration of the Listed Buildings pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

8) Prior to the installation new windows within the existing listed façade, a detailed method statement and specification of the windows (including frame size, fenestration and design together with how they will be fitted and fixed to the historic fabric - appropriate sections, elevations and materials) shall be submitted for approval in writing by the City Council, as Local Planning Authority. This should also include any removal, alteration and repair of the historic fabric.

The works shall then be carried out in accordance with this method statement and specification as part of the development and be completed prior to the first occupation of the commercial units.

Reason - To ensure that a satisfactory interventions into the Listed Building and retention/repair of as much of the Listed fabric as possible along with appropriate use of materials in the restoration of the Listed Buildings pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

9) Prior to the installation new shopfronts within the existing listed façade, a detailed method statement and specification of the shopfronts (including frame size, fenestration and design together with how they will be fitted and fixed to the historic fabric - appropriate sections, elevations and materials) shall be submitted for approval in writing by the City Council, as Local Planning Authority. This should also include any removal, alteration and repair of the historic fabric.

The works shall then be carried out in accordance with this method statement and specification as part of the development and be completed prior to the first occupation of the commercial units.

Reason - To ensure that a satisfactory interventions into the Listed Building and retention/repair of as much of the Listed fabric as possible along with appropriate use of materials in the restoration of the Listed Buildings pursuant to policy EN3 of the

Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

10) Prior to the commencement of works to install any relevant mechanical and electrical (M & E) installations (including CCTV and lighting) attached to any listed fabric, detailed plans, method statement and specification showing the location, profile, fixing, sections and suitable samples, where necessary shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of the removal, alterations and repair of the historic fabric of the building.

The development shall then be carried out in accordance with the details approved and thereafter be retained as approved throughout the life of the development. Should the M & E no longer be required, they should be removed and the elevations should be made good following a scheme previously approved in writing by the City Council, as Local Planning Authority.

Reason:- To ensure the M & E at the development does not harm the Listed structure and is attached appropriately to the historic fabric pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

11) Prior to the commencement of works to the roof (including installation of roof lights) a detailed method statement and specification for the works shall be submitted for approval in writing by the City Council, as Local Planning Authority (including methods for undertaking works, use of materials, fixing details and sections where appropriate).

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Reason - To ensure that a satisfactory interventions into the Listed Building and to ensure that the repair and maintenance work is suitable and appropriate for the restoration of the ventilation stack pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

12) Prior to the commencement of works associated with the new lift cores and staircase to the roof terrace, a detailed method statement and specification for the works shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of the how openings would be created within the listed building in order to minimise unnecessary removal of listed fabric. The approved details shall be implemented as part of the development and be completed prior to the first occupation of the development. Should any parts of the Listed Building become damaged as a consequence of the works then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the installation of the lifts and stairs is appropriate and does not cause and harm or damage to the fabric of the Listed Building and that any damaged

to the building which result as a consequence is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

13) Prior to the commencement of works to existing internal walls, internal brick work and stonework and internal floors within the listed building, a detailed method statement, specification and repair strategy for the works outlined in the Stephen Lavrant Heritage Statement received by the City Council, as Local Planning Authority, on the 24 November 202 shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The works shall be carried out in accordance with this method statement and be completed prior to the first occupation of the development.

Reason - To ensure that the original floor is exposed where possible and areas of new flooring are appropriate and minimise and impact on the listed building pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

14) Prior to the commencement of the extension to the listed building as approved under planning permission 135565/FO/2022, a detailed method statement and specification detailing how the development will be fixed/erected adjacent to the listed building shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of how the interface of the development would be attached to the historic fabric, what historic fabric, if any, needs to be removed/alterd and how the historic fabric will be repaired once the interface has been attached. The method statement should be supported by appropriate sections where necessary.

The approved details shall be implemented as part of the development. Should any parts of the Listed structure become damaged as a consequence of the alterations then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the method used to attach the development to the historic fabric is appropriate and that any damaged to the historic fabric is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

15) Prior to the first occupation of the listed building, a tenant fit out strategy shall be submitted for approval in writing by the City Council, as Local Planning Authority. The shall include details and principles for individual tenants to following including partitions, fixtures and fittings.

The approved details shall be implemented as part of the proposal and thereafter retained and maintained for a long as the listed building is in use as offices.

Reason - To ensure that a satisfactory interventions into the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

Listed Building Consent 135566/LO/2022 'Great Northern Warehouse'

Recommendation Minded to Approve subject to the completion of the legal agreement associated with planning permission 135565/FO/2021

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. Pre application advice has been sought in respect of this matter where early discussions took place regarding the works to the listed building. The proposal is considered to be acceptable and therefore determined within a timely manner.

Condition(s) to be attached to decision for approval

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 18 of the Planning (Listed Buildings and Conservation Areas) Act 1990.

2) The demolition and alteration to Great Northern Warehouse shall not be undertaken before a contract for the carrying out of the building works have been made, and planning permission has been granted for the redevelopment for which the contract provides, and evidence of that contract has been supplied to the City Council as Local Planning Authority.

Reason - In the interests of visual amenity and for the avoidance of doubt, and to ensure that redevelopment of the site takes place following external alterations to the existing building pursuant to policy EN3 of the Manchester Core Strategy and saved DC19 of the Unitary Development Plan for the City of Manchester, and the National Planning Policy Framework.

3) The development hereby approved shall be carried out in accordance with the following drawings and documents:

Drawings

10422-SHP-Z0-A-BD59-Z460-PL-XX-001, 10422-SHP-Z0-A-B5D9-Z460-PL-XX-002, 10422-SHP-Z0-A-B5D9-G100-PL-00-001, 10422-SHP-Z0-A-B5D9-G100-PL-01-001, 10422-SHP-Z0-A-B5D9-G100-PL-02-001, 10422-SHP-Z0-A-B5D9-G100-PL-04-001, 10422-SHP-Z0-A-B5D9-G100-PL-10-001, 10422-SHP-Z0-A-B5D9-G100-PL-12-001, 10422-SHP-Z0-A-B5D9-G100-PL-25-001, 10422-SHP-Z0-A-B5D9-G100-PL-33-001, 10422-SHP-Z0-A-B5D9-G100-PL-RF-001, 10422-SHP-Z0-A-B5D9-G100-SE-AA-001, 10422-SHP-Z0-A-B5D9-G100-SE-BB-001, 10422-SHP-Z0-A-B5D9-G100-EL-EN-001, 10422-SHP-Z0-A-B5D9-G100-EL-EE-001, 10422-SHP-Z0-A-B5D9-G100-EL-EE-002, 10422-SHP-Z0-A-B5D9-G100-EL-EW-001, 10422-SHP-Z2-A-B5D9-G200-PL-00-001, 10422-SHP-Z2-A-B5D9-G200-PL-M0-001, 10422-SHP-Z2-A-B5D9-G200-PL-01-001, 10422-SHP-Z2-A-B5D9-G200-PL-M1-001, 10422-SHP-Z2-

A-B5D9-G200-PL-02-001, 10422-SHP-Z2-A-B5D9-G200-PL-03-001, 10422-SHP-Z2-A-B5D9-G200-PL-04-001, 10422-SHP-Z2-A-B5D9-G200-PL-RF-001, 10422-SHP-Z2-A-B5D9-G200-SE-AA-001, 10422-SHP-Z2-A-B5D9-G200-SE-BB-001, 10422-SHP-Z2-A-B5D9-G200-EL-EN-001, 10422-SHP-Z2-A-B5D9-G200-EL-ES-001, 10422-SHP-Z2-A-B5D9-G200-EL-EE-001, 10422-SHP-Z2-A-B5D9-G200-EL-EW-001, 10422-SHP-Z2-A-B5D9-G200-EL-XX-001, 10422-SHP-Z2-A-B5D9-G332-PL-00-001, 10422-SHP-Z2-A-B5D9-G332-PL-M0-001, 10422-SHP-Z2-A-B5D9-G332-PL-01-001, 10422-SHP-Z2-A-B5D9-G332-PL-M1-001, 10422-SHP-Z2-A-B5D9-G332-PL-02-001, 10422-SHP-Z2-A-B5D9-G332-PL-03-001, 10422-SHP-Z2-A-B5D9-G332-PL-04-001, 10422-SHP-Z2-A-B5D9-G220-XD-XX-001, 10422-SHP-Z2-A-B5D9-G220-DE-XX-001, 10422-SHP-Z2-A-B5D9-G251-DE-00-001, 10422-SHP-Z2-A-B5D9-G251-DE-00-002, 10422-SHP-Z2-A-B5D9-G251-DE-00-003, 10422-SHP-Z2-A-B5D9-G251-DE-00-004, 10422-SHP-Z2-A-B5D9-G251-DE-01-001, 10422-SHP-Z2-A-B5D9-G251-DE-01-002, 10422-SHP-Z2-A-B5D9-G251-DE-01-003, 10422-SHP-Z2-A-B5D9-G251-DE-02-001, 10422-SHP-Z2-A-B5D9-G251-DE-02-002, 10422-SHP-Z2-A-B5D9-G251-DE-XX-001, 10422-SHP-Z2-A-B5D9-G251-DE-XX-002, 10422-SHP-Z2-A-B5D9-G240-DE-00-001, 10422-SHP-Z2-A-B5D9-G240-DE-00-002, 10422-SHP-Z2-A-B5D9-G240-DE-RF-001, 10422-SHP-Z2-A-B5D9-G240-DE-RF-002, 10422-SHP-Z2-A-B5D9-G240-DE-RF-003, 10422-SHP-Z2-A-B5D9-G240-DE-RF-004, 10422-SHP-Z2-A-B5D9-G240-DE-RF-005, 10422-SHP-Z2-A-B5D9-G321-DE-XX-001, 10422-SHP-Z2-A-B5D9-G321-DE-XX-002, 10422-SHP-Z2-A-B5D9-G321-DE-XX-003, 10422-SHP-Z2-A-B5D9-G321-DE-XX-004, 10422-SHP-Z2-A-B5D9-G321-DE-XX-005, 10422-SHP-Z2-A-B5D9-G321-DE-XX-006, 10422-SHP-Z2-A-B5D9-G321-DE-XX-007, 10422-SHP-Z2-A-B5D9-G321-DE-XX-008, 10422-SHP-Z2-A-B5D9-G321-DE-XX-009, 10422-SHP-Z2-A-B5D9-G321-DE-XX-010 and 10422-SHP-Z2-A-B5D9-Z330-SC-XX-001

Received by the City Council, as Local Planning Authority, on the 24 November 2022

Supporting Information

Design & Access Statement prepared by SimpsonHaugh, Public Realm Strategy prepared by Planit.IE, Façade Lighting Strategy prepared by Light Bureau, Stage 2 Civil and Structural Report prepared by Curtins, Condition Surveys prepared by Aura and Quadriga, Archaeological Desk Based Assessment and covering note prepared by Salford Archaeology and Heritage Report prepared by Stephen Levrant.

The above documents were received by the City Council, as Local Planning Authority, on the 24 November 2022

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy.

4) Prior to the commencement of works to remove the following, as indicated within the heritage statement prepared by Stephen Levrant stamped as received by the City Council, as Local Planning Authority, on the 24 November 2022, a detailed method statement shall be submitted detailing how the fabric will be removed together how the remaining historic fabric will be protected and repaired once removal takes place.

This condition applies to the following works:

- (a) Removal of modern extensions (late 1900s – early 2000s);
- (b) Lighting/CCTV;
- (c) Signage;
- (d) Vehicle ramps and link building;
- (e) Modern canopy; and
- (f) Non original rainwater goods.

The removal works shall be carried out in accordance with the approved details. Should any other parts of the Listed structure become damaged as a consequence of the removal/alteration work then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the method used to remove historic fabric is appropriate and that any damaged to the historic fabric is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

5) Prior to the commencement of repair works to the exterior masonry and stonework, a detailed method statement and specification (including material specification) for the removal, alteration, repair, re-pointing, cleaning and replacement works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Best endeavours shall be made to use salvage brick work and stonework and other materials wherever possible in the works associated with the development and arrange safe storage. Should it not be possible to use salvage material in the restoration then notification shall be made to the City Council, as Local Planning Authority, prior to the starting of the works. Suitable materials shall then be agreed with the City Council, as Local Authority, and used where necessary.

Reason - To ensure that a satisfactory restoration and repair of the exterior of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

6) Prior to the commencement of works to repair the original rainwater goods to the listed building, a detailed method statement and specification shall be submitted for approval in writing by the City Council, as Local Planning Authority. The repair to the rainwater goods shall be carried out in accordance with this method statement and be completed prior to the first occupation of the development.

Reason - To ensure the rainwater goods are of an acceptable appearance and there is no resulting harm to the Listed building pursuant to policy EN3 of the Manchester Core Strategy (2012).

7) Prior to the repair of windows within the existing listed façade, a detailed method statement and specification for the window repairs shall be submitted for approval in writing by the City Council, as Local Planning Authority. This should also include any removal, alteration and repair of the historic fabric.

The works shall then be carried out in accordance with this method statement and specification as part of the development and be completed prior to the first occupation of the commercial units.

Reason - To ensure that a satisfactory interventions into the Listed Building and retention/repair of as much of the Listed fabric as possible along with appropriate use of materials in the restoration of the Listed Buildings pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

8) Prior to the installation of new windows within the existing listed façade, a detailed method statement and specification of the windows (including frame size, fenestration and design together with how they will be fitted and fixed to the historic fabric - appropriate sections, elevations and materials) shall be submitted for approval in writing by the City Council, as Local Planning Authority. This should also include any removal, alteration and repair of the historic fabric.

The works shall then be carried out in accordance with this method statement and specification as part of the development and be completed prior to the first occupation of the commercial units.

Reason - To ensure that a satisfactory interventions into the Listed Building and retention/repair of as much of the Listed fabric as possible along with appropriate use of materials in the restoration of the Listed Buildings pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

9) Prior to the commencement of works to open up the original hoist ways (and install windows and balconies) a detailed method statement shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first use of the development.

Reason - To ensure that a satisfactory restoration and repair of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

10) Prior to the commencement of works to the internal columns and girders, a detailed method statement and specification (including material specification) for the exposure of the columns, alteration, repair and cleaning shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Reason - To ensure that a satisfactory restoration and repair of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

11) Prior to the installation and repair of new shopfronts (plaza and ground floor units along Deans Street) and carriage ramp opening (first floor level) within the existing listed façade, a detailed method statement and specification/repair strategy of the shopfronts (including frame size, fenestration and design together with how they will be fitted and fixed to the historic fabric - appropriate sections, elevations and materials) shall be submitted for approval in writing by the City Council, as Local Planning Authority. This should also include any removal, alteration and repair of the historic fabric.

The works shall then be carried out in accordance with this method statement and specification as part of the development and be completed prior to the first occupation of the commercial units.

Reason - To ensure that a satisfactory interventions into the Listed Building and retention/repair of as much of the Listed fabric as possible along with appropriate use of materials in the restoration of the Listed Buildings pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

12) Prior to the commencement of works to:

- (a) the new glazed extension to the south elevation;
- (b) new external lift core;
- (c) glazed link bridge; and
- (d) roof atrium and photovoltaics.

to the listed building as approved under planning permission 135565/FO/2022, a detailed method statement and specification detailing how the development will be fixed/erected adjacent to the listed building shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of how the interface of the development would be attached to the historic fabric, what historic fabric, if any, needs to be removed/alterd and how the historic fabric will be repaired once the interface has been attached. The method statement should be supported by appropriate sections where necessary.

The approved details shall be implemented as part of the development. Should any parts of the Listed structure become damaged as a consequence of the alterations then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the method used to attach the development to the historic fabric is appropriate and that any damaged to the historic fabric is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

13) Prior to the commencement of repair works to the Jack arches, a detailed method statement and specification (including material specification) for the removal, alteration, repair, cleaning, replacement and fire protection works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Best endeavours shall be made to use salvage and re-use existing features, brickwork and other materials wherever possible in the works associated with the development. Should it not be possible to use salvage material in the restoration then notification shall be made to the City Council, as Local Planning Authority, prior to the starting of the works. Suitable materials shall then be agreed with the City Council, as Local Authority, and used where necessary.

Reason - To ensure that a satisfactory restoration and repair of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

14) Prior to the commencement of works associated with new lifts and stair cores, detailed method statement and specification for the works shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of the how openings shall be created within the listed building floors in order to minimise unnecessary removal of listed fabric. The approved details shall be implemented as part of the development and be completed prior to the first occupation of the development. Should any parts of the Listed Building become damaged as a consequence of the works then such damage should be made good following a method of works previously agreed in writing by the City Council, as Local Planning Authority.

Reason - To ensure the installation of the lifts is appropriate and does not cause and harm or damage to the fabric of the Listed Building and that any damaged to the building which result as a consequence is repaired to a satisfactory standard pursuant to policy EN3 of the Manchester Core Strategy (2012) and saved policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

15) Prior to the commencement of works associated with the installation of the new internal partitions, circulation cores and mezzanines of the development, a detailed method statement and specification (including sections, elevations and materials) in association with the installation works (including the removal, alteration and repair of the historic fabric), shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The works shall then be carried out in accordance with this method statement and specification as part of the development and be completed prior to the first occupation of the development.

Reason - To ensure that a satisfactory interventions into the Listed Building and retention/repair of as much of the Listed fabric as possible along with appropriate use of materials in the restoration of the Listed Buildings pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

16) Prior to the commencement of works to install any relevant mechanical and electrical (M & E) installations (including CCTV and lighting) attached to any listed fabric, detailed plans, method statement and specification showing the location, profile, fixing, sections and suitable samples, where necessary shall be submitted for approval in writing by the City Council, as Local Planning Authority. This shall include details of the removal, alterations and repair of the historic fabric of the building.

The development shall then be carried out in accordance with the details approved and thereafter be retained as approved throughout the life of the development. Should the M & E no longer be required, they should be removed and the elevations should be made good following a scheme previously approved in writing by the City Council, as Local Planning Authority.

Reason:- To ensure the M & E at the development does not harm the Listed structure and is attached appropriately to the historic fabric pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995).

17) Prior to the commencement of repair works to the original concrete staircases (each corner of warehouse), a detailed method statement and specification (including material specification) for the removal, alteration, repair, cleaning and replacement works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Best endeavours shall be made to use salvage and re-use existing features, stone/brickwork and other materials wherever possible in the works associated with the development. Should it not be possible to use salvage material in the restoration then notification shall be made to the City Council, as Local Planning Authority, prior to the starting of the works. Suitable materials shall then be agreed with the City Council, as Local Authority, and used where necessary.

Reason - To ensure that a satisfactory restoration and repair of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

18) Prior to the commencement of repair works to the interior masonry and stonework, a detailed method statement and specification (including material specification) for the removal, alteration, repair, re-pointing, cleaning and replacement works shall be submitted for approval in writing by the City Council, as Local Planning Authority.

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Best endeavours shall be made to use salvage brick work and stonework and other materials wherever possible in the works associated with the development. Should it not be possible to use salvage material in the restoration then notification shall be made to the City Council, as Local Planning Authority, prior to the starting of the works. Suitable materials shall then be agreed with the City Council, as Local Authority, and used where necessary.

Reason - To ensure that a satisfactory restoration and repair of the exterior of the listed building in order to ensure that the historic fabric is retained where possible and appropriate materials used in the restoration of the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

19) Prior to the commencement of works to the roof (including installation of roof lights, PV panels and repairs) a detailed method statement and specification for the works shall be submitted for approval in writing by the City Council, as Local Planning Authority (including methods for undertaking works, use of materials, fixing details and sections where appropriate).

The approved details and method statement shall be implemented as part of the development and completed prior to the first occupation of the development.

Reason - To ensure that a satisfactory interventions into the Listed Building and to ensure that the repair and maintenance work is suitable pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

20) Prior to the first occupation of the listed building, a tenant fit out strategy shall be submitted for approval in writing by the City Council, as Local Planning Authority. The shall include details and principles for individual tenants to following including partitions, fixtures and fittings.

The approved details shall be implemented as part of the proposal and thereafter retained and maintained for a long as the listed building is in use as offices.

Reason - To ensure that a satisfactory interventions into the Listed Building pursuant to policy EN3 of the Manchester Core Strategy (2012) and extant policy DC19 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF/NPPG.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 135565/FO/2022 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

The following residents, businesses and other third parties in the area were consulted/notified on the application:

**Highway Services
Environmental Health
Neighbourhood Team Leader (Arboriculture)
MCC Flood Risk Management
Work & Skills Team
Greater Manchester Police
Historic England (North West)
Transport For Greater Manchester
Greater Manchester Archaeological Advisory Service
United Utilities Water PLC
Health & Safety Executive (Fire Safety)
Manchester Airport Safeguarding Officer
National Amenity Societies
Greater Manchester Ecology Unit
Planning Casework Unit**

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties:

Relevant Contact Officer : Jennifer Atkinson
Telephone number : 0161 234 4517
Email : jennifer.atkinson@manchester.gov.uk

